



Mendocino County

ADA Comprehensive Access Plan for the County Maintained Road System

July 16, 2010



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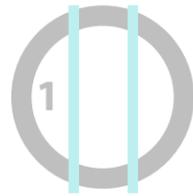
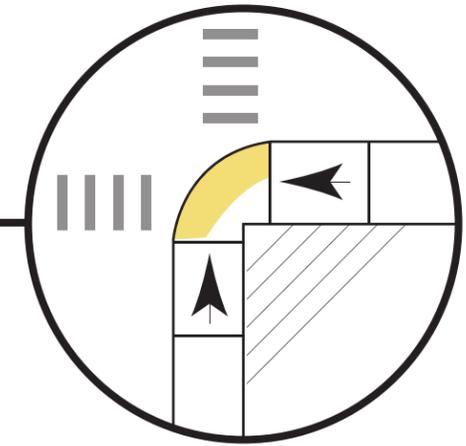


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Introduction



This plan as commissioned by Mendocino County Department of Transportation is intended to address aspects of the county maintained road system that are appropriate for pedestrian infrastructure features to provide path of travel in compliance with the federal Americans with Disabilities Act and applicable provisions in the California Building Code.

Components in the plan are comprehensive in nature and designed to result in a document suitable for the user to understand Mendocino County's current status relative to disabled access barriers and provide a planning tool for implementing work scope in the road right of way to remove barriers. There are a number of interrelated issues and concepts folded into this document, described below:

- Identifying road conditions for pedestrian infrastructure in the public right of way including a focus on key unincorporated communities where population and development density is appropriate for urban pedestrian infrastructure.
- Document conditions found with categorization for improving the right of way based on development density and road types per the County's road standards.
- Evaluate current policies procedures and practices relative to disabled access barrier removal then recommend documentation methods for work already performed and future work to improve pedestrian infrastructure in road right of way.
- Evaluate and report on current ADA transition plan status for disabled access barrier removal in road right of way, most notably in Town of Mendocino town area.
- Evaluate development ordinances and typically required project approval conditions to identify potential contributory funding sources for road network impacts.
- Evaluate transportation planning concepts used for implementing pedestrian route improvements.
- Provide training for Department of Transportation personnel to perform ADA assessments in the field. (Training will not be part of the plan document)

The report overview and methodology section will explain the plan components.

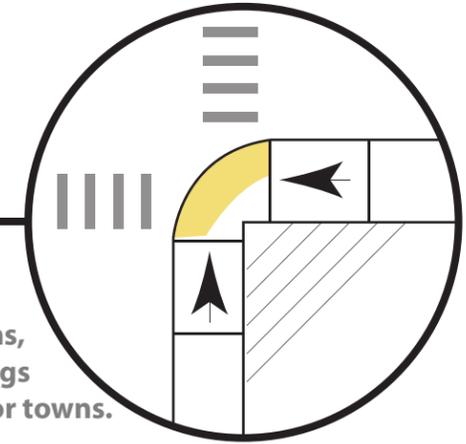
Sally Swanson Architects extends our gratitude to Mendocino County's Department of Transportation personnel for their effort in providing information to us and for coordinating this project, their assistance was instrumental in preparing this document. The Department of Transportation also deserves recognition for incorporating training into the overall project for Staff to have a better understanding of disabled access requirements.

Key County staff members on the project:

Bob Parker, Assistant Director of Transportation
Tom Peters, Deputy Director of Transportation, Land Improvement
James Sookne, Engineering Technician II
Keri Rynearson-Garrity, Surveyor I



Overview & Methodology



Mendocino County's maintained road system has approximately 1,020 centerline roadway miles including features such as bridges, roadside drainage systems, pedestrian crossings, curb gutter and sidewalks in some urbanized areas, traffic controls, and bikeways. Most of the road network is in rural open space settings with a very small portion having features that are considered urban in nature. The urban type features are present primarily in unincorporated communities or towns.

In order to understand how the road system relates to infrastructure improvements, knowledge of Mendocino County's General Plan and Road and Development Standards is necessary. The General Plan and the Road Standards have certain goals and policies stated relating to road network systems and related features. If the Reader is not familiar with road systems, it is recommended to review the "Selected references from the General Plan and the Road Standards" section in this report before continuing.

Pedestrian infrastructure is typically required in urban areas by County ordinance and various development standards for new work and when alterations or additions to existing properties exceed a certain amount. For this plan, the first tasks included reviewing available documents and identifying what will be considered urban versus rural. Then other parameters and features were chosen that define character and conditions within the existing road system relative to development planning concepts, transportation planning concepts, and their attendant requirements. Within general public policy perspectives, the approach to making this plan a forward looking planning document must include whether the pedestrian infrastructure recommendations made are feasible and appropriate.

For compliance with the Americans with Disabilities Act, government agencies must make their programs accessible to and useable by disabled persons. Providing pedestrian infrastructure is a known program provided by the County and must be accessible in compliance with construction standards in the ADA and also applicable California construction standards in the California Building Code. Existing pedestrian infrastructure constructed prior to the ADA becoming law are required to "transition" into compliance, this plan will afford guidance to that end. Whether a particular existing pedestrian feature such as a sidewalk complies with applicable construction standards is a basis for review and recommendations in this report.

A focused review for urban nature conditions was done for the following communities, considered "key" areas with urban population density. Relative factors to consider a community "key" include these characteristics; adjacent to an existing city, within an urban planning area, recognizable as a community near a larger urban area, recognizable as a distinct separate rural town, or a known travel destination. The list is in no particular order but trends geographically from southeast counterclockwise around the county.

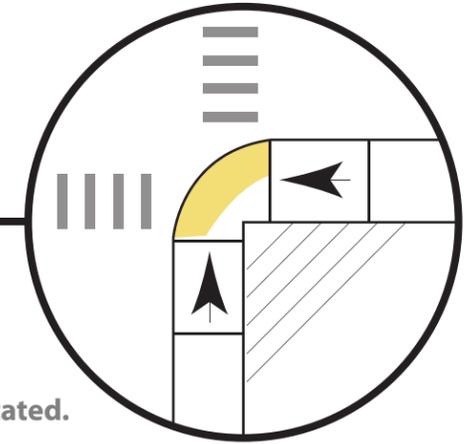
Hopland, including Old Hopland area (rural town)
Ukiah area outside the city limits (population density, adjacent to a city, & urban planning area)
Talmage (community & urban planning area)
Calpella (community & urban planning area)
Potter Valley (rural town)
Redwood Valley (community)
Willits area outside the city limits (population density, adjacent to a city, & urban planning area)
Brooktrails Township (community & urban planning area)
Covelo (rural town)
Laytonville (rural town)
Westport (rural town)

Fort Bragg area outside the city limits (population density, adjacent to a city, urban planning area, & travel destination)
Gualala (rural town & travel destination)
Elk (rural town & travel destination)
Comptche (rural town)
Town of Mendocino (population density, rural town, urban planning area, & travel destination)
Caspar (rural town)
Albion (rural town)
Boonville (rural town & travel destination)



Overview & Methodology

Continued



After the information was gathered on existing conditions, typical solutions were developed with associated considerations such as surface water drainage stated.

Other plan tasks include evaluations for development policies procedures and practices, development ordinances, and implementation concepts for improving the County's road network. In order to fold in these tasks, a categorizing system was developed that can assist with prioritizing relative importance for where improving pedestrian infrastructure is most appropriate. The four categories are briefly described below in a broader sense. Fuller descriptions for aspects relative to the characteristics are in the following report section titled "Community Categories".

Category 1

- adjacent to an existing city
- within an urban planning area
- recognizable as a community near a larger urban area
- may be a known travel destination or provide significant visitor serving facilities
- existing road types are predominantly urban style

Considered as the highest rank for potential development and population density in the near future. Examples are the urban density areas adjacent to the cities of Ukiah and Fort Bragg.

Category 2

- within an urban planning area
- recognizable as a community near a larger urban area
- existing road types are predominantly urban style OR planned to be urban style

This is the next lower rank for future potential development as the urban influence is less. A good example is the small community of Talmage near and southeast of Ukiah.

Category 3

- recognizable as a distinct separate rural town
- may be a known travel destination or provide significant visitor serving facilities
- existing road types are generally mixed with no predominant style throughout the town area

These separate communities each will have their own local pressures to drive development but generally are constrained by local economy or environment causing lower potential for future development yet may experience significant pressure to develop given the right circumstances. Examples are Boonville, and Town of Mendocino.

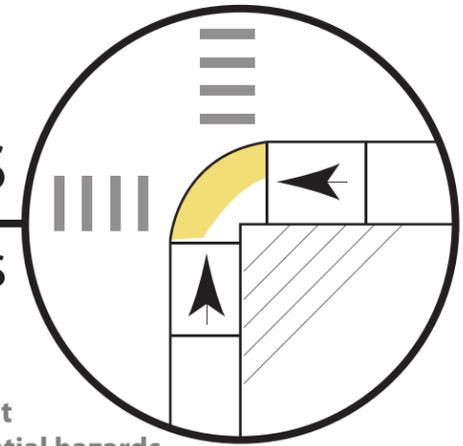
Category 4

- recognizable as a distinct separate rural town
- existing road types are predominantly rural

This is considered the lowest potential for future development due to very low development density or very remote location. Good examples are Albion and Comptche.

Policies and Procedures Review with Recommendations

Review with recommendations



From time to time, citizens make requests for modifications to existing features in the road right-of-way. Most common requests are made via complaint about various features; traffic light timing is an example of the more prevalent citizen concerns. Sometimes the citizen relates conditions that they perceive as potential hazards. At this time, no formal complaint system exists for the general public or a request system for concerns the citizen may have regarding features in the right-of-way.

Currently requests and complaints are processed by Staff with each request or complaint routed to the Division or person that either has the job responsibility for the citizen's concern, or may know the most about the issue at hand. To date this informal system has worked quite well as Staff is knowledgeable and capable, and routing has been effective to the person or Division responsible for the issues at hand. As the County grows in population with associated traffic increase and more roads are constructed and with new staff or at the time that senior staff members retire, the collective memory and skills for maintaining the current informal process will be lost without established procedures.

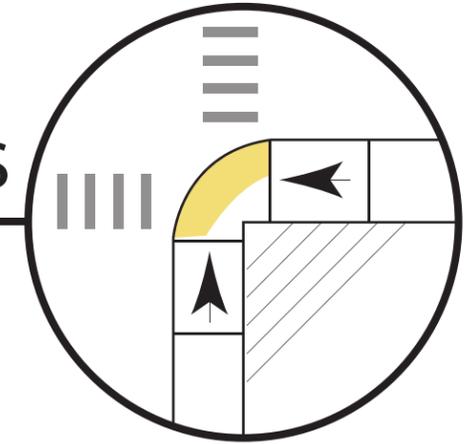
A recommended policy for formalizing the complaint and request system should include the following:

- The public is to have informational notices in print, and electronic media such as the County website, that complaints and requests can be made regarding citizen's concerns for the County maintained right-of-way.
- An explanation on the form(s) and on the website for which division or person has authority for the various department functions.
- Clear instructions should be included on the form(s) and on the website explaining what kind of information is necessary for the Department to reconcile the complaint or request.
- Requests for modifications to County maintained right-of-way infrastructure will be considered on the basis of traffic safety, alternate transportation mode safety (such as bicycles), pedestrian safety, and disabled access.
- Form(s) should be developed that gather sufficient information on the person(s) making request or complaint, and the issues at hand.
- Typical complaint or request categories should be outlined on the form for the citizen to choose. This will reduce misunderstandings for citizen's concerns.
- The process for how the request or complaint will be routed, decision parameters, and response anticipated should be stated on the form(s).
- Forms for requests and complaints are made available on the website.
- Notice at public counters that printed forms are available for requests and complaints.
- Sufficient forms are in stock at public counters in Department of Transportation offices.
- Periodically the forms and system should be reviewed for performance and effectiveness with updates and revisions made that reflect the most current policies, programs, and services applicable.

It is recommended that reasonable budget amounts are allocated for citizen requests for work performed in the road right-of-way. Work categories applicable are typically comprised of repairing older curb ramps and broken sidewalk portions.

A significant and important amount of complaints and requests will involve disabled access issues. Current federal laws require a formal process for disabled access complaints and requests commonly called in those laws as "grievance procedure". The following pages are a typical outline for the grievance procedure that in concept can be revised to also accommodate other complaints or requests.

Policies and Procedures Review with Recommendations



(Model program) Grievance Procedures

A. Overview

These procedures cover all grievances and/or complaints filed under the Americans with Disabilities Act of 1990, relating to any program, service or activity managed by the Mendocino County. The procedures do not deny the right of the complainant to file formal grievances and/or complaints with other state or federal agencies or to seek private counsel for complaints alleging discrimination or unaccommodated barriers to access. Every effort will be made to resolve grievances and/or complaints informally at the County-level. The option of informal mediation meeting(s) between the affected parties and a designated mediator may be utilized for resolution. Intimidation or retaliation of any kind is prohibited by law.

B. Procedures

1. Any individual, group of individuals or entity that believes they have been subjected to discrimination or faced with unaccommodated barriers to access as defined by the ADA may file a written complaint with (person's name), the County ADA Coordinator. A formal complaint must be filed within 180 calendar days of the alleged occurrence. The County will not officially act or respond to complaints made verbally.

2. Upon receiving the written complaint, the County ADA Coordinator, in consultation with other County departments/offices, will determine its jurisdiction, acceptability, need for additional information and the investigative merit of the grievance and/or complaint.

3. Based on initial consultation, the County may determine that an external entity conduct the investigation in lieu of the County ADA Coordinator. If the grievance and/or complaint is against a sub-recipient, consultant or contractor, under contract with the County, the appropriate department and/or division shall be notified of the complaint within 15 calendar days, and the County will determine whether it will investigate the complaint, or request that a third party conduct the investigation.

4. Once the County decides its course of action, the complainant and the respondent will be notified in writing of such determination within five (5) calendar days. The grievance and/or complaint will be logged into the records of the ADA Coordinator.

5. When Mendocino County assumes investigation of the grievance and/or complaint, the County will provide the respondent with the opportunity to respond to the allegations in writing. The respondent will have ten (10) calendar days upon receipt to furnish the County with his/her response to the allegations.

6. Within 60 days of receipt of the grievance and/or complaint, the ADA Coordinator or third-party investigator will prepare a written investigative report for the appropriate Department Director and the County Manager. The report shall include a narrative description of the incident, identification of persons interviewed, findings and recommendations for disposition.

7. The recommendation shall be reviewed by the County Counsel's office, which may discuss the report and recommendations with the ADA Coordinator and other appropriate departmental staff. The report will be modified as needed and finalized for its release to the parties.

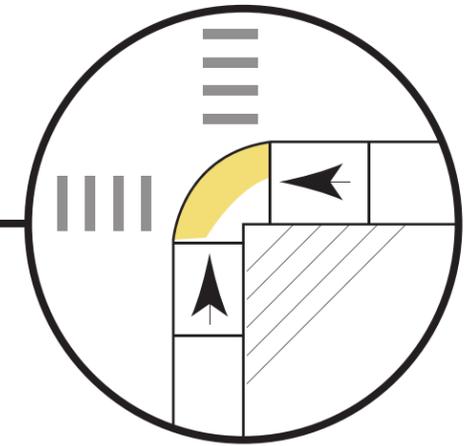
8. Once the investigative report becomes final, briefings will be scheduled with each party within 15 days. Both the complainant and the respondent shall receive a copy of the report during the briefings and will be notified of their respective appeal rights.

9. If the complainant or respondent is not satisfied with the results of the investigation of the alleged discriminatory practice(s) he or she shall be advised of their rights to appeal the decision to the U.S. Department of Justice. The complainant has 180 calendar days after the appropriate agency's final resolution to appeal to USDOJ. Unless new facts not previously considered come to light, reconsideration of the final determination by the investigating agency will not be available.

10. The ADA Coordinator will maintain an annual log of grievances and/or complaints, which will contain the following information for each complaint filed:

- The name and address of the person filing the grievance/complaint
- The date of the grievance/complaint
- The basis of the grievance/complaint
- The disposition of the grievance/complaint
- The status of the grievance/complaint

Development Ordinance Review



Continued

Development ordinance review with recommendations

Ordinances that pertain to land use and standards afford for orderly and consistent development growth during the time that the ordinance is in effect. The County land use ordinances have been relatively stable for about a decade providing a measure of consistency yet there are noticeable differences in newer improved right-of-way meeting urban standards for apparently equivalent project types. The most likely basis for differences is how the right-of-way improvement standards are applied during the project permit processing. In the last few years, the County has reorganized for project permit processing and now has a permit center that handles most all construction and development permits in a centralized location and Staff is working to streamline processes for everyone's benefit. Consistent standards application is a goal of the teams participating in the permit center.

Ordinances are comprehensive and well written for development requirements based on land uses. Ordinances have been compiled in the standard chapter part and section format found throughout the United States and should be familiar in nature to most experienced users such as professional developers. The ordinances are not very user friendly for the first time or occasional user prompting the permit center to publish explanatory materials and informational handouts including a very well done process document available on the County website and in print at the permit center. It is notable that the ordinances in the "county code" do not directly address standards for road right-of-way improvements based on project types except for the singular ordinance that adopts the current Department of Transportation "Road and Development Standards".

Understanding that certain latitude is required for professional judgment in applying the road standards, the language for implementing road improvements as a project requirement is not direct and specific. Typically the process to have road or "off site" improvements as part of a project relies on the use permit process with the Planning Commission approval for a project including approval conditions that incorporate road right-of-way improvements. The "ministerial" type projects not subject to discretionary permit processes are not currently directly addressed in the road standards. Discussion within funding program descriptions will address this issue.

In order to implement pedestrian infrastructure improvements, funding mechanisms need to be considered, both existing programs and potential new or altered existing methods. A number of potential strategies and potential funding sources are described following:

Existing programs:

Capital Improvement program:

Projects are primarily funded via taxing distributions from the State of California typically for road projects and some buildings. Currently the State has reduced distributions to Counties due to the economic climate and this program will not be sufficient to expand work desired for pedestrian projects.

Redevelopment Agency:

Funding is by tax increments and grant sources. The Redevelopment Plan includes pedestrian infrastructure and road projects that will implement improvements mostly on North State Street in Ukiah area and improvements on South State Street in Ukiah area. Calpella is also part of the redevelopment project area and could benefit from projects as well. It is recommended to investigate the redevelopment implementation strategy for potential opportunities for funding more pedestrian infrastructure. Success will be dependent upon balancing the other goals in the plan with increase in pedestrian projects and the recent State funding levy reducing tax increments available.

Potentially additional redevelopment project areas could be considered in other rural community areas or towns. The tax increment system has been quite successful in providing significant improvements over time in the last 50 years of redevelopment agencies across the State.

Building Permits, with discretionary approval process:

Right-of-way improvements are required as part of the project and paid for by the project developer. The extent for the improvements is typically a direct result of an environmental assessment or impact that requires mitigation to acceptable levels. This process is currently in place and is quite customary throughout the State. Since development projects many times are not adjacent, continuity in pedestrian infrastructure routes cannot be realized until enough adjacent properties are developed. Since the County does not fund the improvements on these larger projects, public funding sources can be utilized for other projects such as CIP.

Building Permits, with ministerial approval process:

At this time projects that do not have discretionary processes that go before an approval board, such as the Planning Commission, do not have precise standards for requiring "off site" improvements. These projects are known as "ministerial" where the project must meet codified standards and regulations by federal, state, and local laws to gain approval. It is customary throughout the state to require "off site" improvements installed and paid for by the developer based upon the project type and scope. Project scope is usually called the "trigger" for requiring developer paid improvements. Typical project categories to consider are as follows:

New buildings:

Since new buildings generally will intensify traffic for vehicles and pedestrians, it is recommended that street and pedestrian infrastructure improvements be made mandatory. Commercial buildings are the use types that should be mainly considered. Residential with high density such as apartments or multiple unit complexes should be included as well. Single family residential single lot projects are candidates for mandating street and pedestrian improvements in urban or urbanizing areas, however the additional costs to the project can be quite unpopular requiring careful consideration if this use type will be mandated for "off site" improvements.

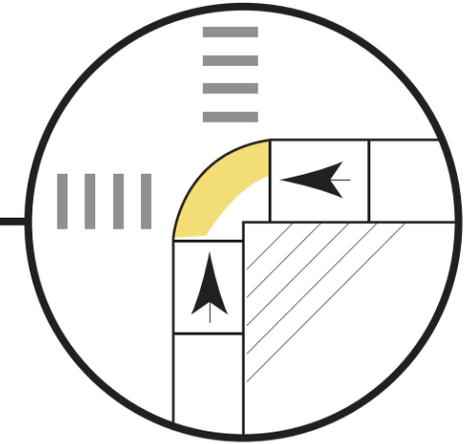
Additions:

Additions to existing buildings of all types should be considered for requiring street and pedestrian improvements based on a size or percentage increase. Throughout the state there are varying amounts used in different jurisdictions. The most common "triggers" are 50% of existing building size or 50% of current construction value considering all improvements to the existing site and buildings. Lower percentages can be considered as well. Another commonly used "trigger" is by project addition size in square feet. There are many variations for certain addition sizes to consider, the most common are 200 square feet to 500 square feet or larger. Commercial use buildings and multiple unit residential are the main types to consider. Requiring "off site" improvements for single family home additions based on a percentage or size is quite unpopular and needs careful consideration for what will "trigger" mandating right-of-way improvements.



Development Ordinance Review

Continued



Alterations:

Alteration projects can be candidates for mandating “off site” improvements. The basis for making this project category a mandatory type for street and pedestrian infrastructure improvements is use intensification. For example a low intensity use type such as an industrial warehouse and office building is converted to office suites increasing occupant loads and traffic. Careful crafting the language and “triggers” is necessary so the additional project costs do not result in a development disincentive to the project type.

Potential programs:

There are a couple of possible partnering type sources such as ARRA (American Recovery and Reinvestment Act) and similar. Currently the County is pursuing various known opportunities to provide additional funding from these sources. Other possible programs to consider are:

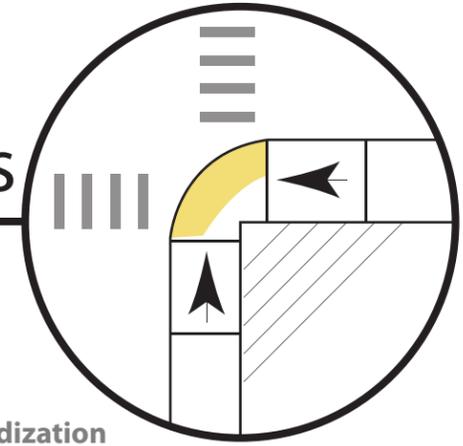
Improvement Districts:

Similar in concept to forming an undergrounding district with the PUC, these districts are formed for the purpose of providing specific improvements for the district benefit to citizens and properties. Various mechanisms are used with the most popular being a property tax payment pass-through to the district for funding the desired improvements. The process to form an improvement district can take some time due to the procedures required including validation studies with reports, negotiating the terms for benefits and funding, public hearings, and ratification processes with the attendant government organizing for funding.

Budget Accrual:

The concept for this funding method relies on fee based pass-through to accumulate moneys into a fund that has specific and limited purpose. A good example is a small fee attached to building permits deposited to the fund each time a building permit is paid. Other fee for service types can be considered if the fee is relative to the fund purpose. Establishing the fund requires validation in the budget process and incorporation to the master fee schedule, and may require public hearings with Board ratification. Developing the connection for the fee to the fund purpose requires in depth analysis and carefully crafting the fee amount so it is commensurate to the service fee and the fund purpose.

Transportation Planning Concepts review with recommendations



Transportation planning concepts for implementing pedestrian route improvements in the County Maintained Road System are comprehensively included in the “Road and Development Standards” and the concepts are consistent with professional engineering practice knowledge base. It is recognized that standardization is necessary for consistent and orderly development in the County, but to meet the General Plan goal for maintaining “community character” there should be more options available. Small rural communities over time have developed in different manners and features in different locales within the urbanized areas. The mix of pedestrian infrastructure types from paved shoulder to curb gutter sidewalk with varying types of surface water drainage controls including swales or curbs sets the visual character in the town. Preserving the mix is desired.

Consideration should be given to affording a variety of urban standard styles in the road profiles consistent with existing improvements. Options can be shown by alternate road designs inserted to the current standards with cross references to typical designs or directly incorporated to each typical design. Changing the existing pedestrian infrastructure type or style should be discouraged when applying the standards to any specific project.

Town of Mendocino is a specific challenge for orderly development due to the community town area being a historic place requiring preservation of the historic nature and features in the historic town. In order for pedestrian infrastructure to be implemented in an orderly manner; the Historic Review Board, Planning, and Department of Transportation need to develop design standards for the historic town area consistent with historic preservation that also affords safe pedestrian use for disabled persons. Solutions will be difficult and may require extensive effort.

There is increasing knowledge for disabled access designs in the right-of-way that are recognized as best practices and gaining popularity. County Staff is aware and using these best practices for plan reviews and for County project designs. It is recommended these best practices be incorporated to the Road and Development Standards where possible.

Selected References

SELECTED REFERENCES FROM THE GENERAL PLAN AND THE ROAD STANDARDS

The following selections are the General Plan goals and policies for the road network and related features for pedestrian infrastructure involved. Commentary for relationship to this plan document is provided in *italics*.

GENERAL PLAN; DEVELOPMENT GOALS

These are the main goals for transportation systems in general, relative to other planning issues.

Goal DE-8 (Transportation) A balanced and coordinated transportation system that:

- Is an integrated and attractive part of each community.
- Is functional, safe and pleasant to use, and supports emergency services.
- Provides a choice of modes accessing and connecting places frequented in daily life.
- Promotes compact development and infrastructure efficiencies.
- Is consistent with principles of sustainability and conservation of resources.
- Is not solely dependent on the continuation of fossil fuel resources.
- Can be maintained, used, and justified if available energy sources change during the duration of the General Plan.

This goal provides the transportation system continuity.

Goal DE-9 (Road Systems) A countywide road system that provides safe, efficient and attractive access, coordinated with interstate, state, local and areawide systems.

Bicycles are a recognized element in the transportation network.

Goal DE-10 (Pedestrian & Bicycle) Functional, safe and attractive pedestrian and bicycle systems coordinated with regional and local transportation plans and other transportation modes.

GENERAL PLAN; Infrastructure (Facilities and Services) Policies:

As development occurs, all types of transportation must be included in the right-of-way with the project developer responsible to fund improvements relative to the project. Grant funds are identified as they are sometimes available for specific uses or programs.

Policy DE-126: Provide for multiple transportation modes and functions within transportation corridors and rights-of-way constructed by project developers or using appropriate grants funding.

It is recognized that budget constraints are severe in the current state of affairs with State of California as the State, via various tax collecting mechanisms, provides significant funds for local road system maintenance. Grant funding may be available for specific uses or programs.

Policy DE-127: The County's transportation policies and funding priorities shall emphasize use of multiple transportation modes with the acknowledgment that general transportation operation and maintenance funding is barely adequate for existing roadway safety maintenance. Emphasis should be placed on securing additional grant funds to support multimodal improvements in the right-of-way.

This policy identifies the user types and includes disabled persons for infrastructure consideration.

Policy DE-128: Ensure that transportation infrastructure accommodates the safety and mobility of motorists, pedestrians, bicyclists, and persons in wheelchairs.

It is important to have funding mechanisms available for larger scale projects when development at large in an area impacts the current road system necessitating upgrade work.

Policy DE-131: Development impact fees, assessments, and other secured funding sources may be required to fund transportation improvements to provide an adequate transportation system or offset transportation impacts.

Legal entitlements for property owners to have state highway access requires cooperation by the County to ensure Caltrans retains the legal controls necessary for state highways. The action item identifies the highways in Caltrans jurisdiction.

Policy DE-136: The County will ensure that development projects which propose direct access to a state highway have legal entitlements for such access.

Action Item DE-136.1: The County will refer to Caltrans all development applications which propose direct access to a state highway. Affected roadways that need to meet the most current Caltrans requirements include all or portions of the following:

- State Route 1
- State Route 20
- Hwy 101
- State Route 128
- State Route 253
- State Route 162

GENERAL PLAN; Road Systems Policies

Road types are an integral part of transportation planning. Typically road networks are ranked from higher order such as the County's regional transportation system with connections to more local roads by decreasing use intensity. Road system types include various considerations relative to land development density and land uses served by the system. Mendocino County's road types are identified by a name title in sequence from most intense use (or traffic count per day basis) to the least.

Policy DE-137: Develop and improve a roadway system that facilitates orderly development and serves the multiple needs of existing and future development. The following are the roadway types defined by the County. A listing of roadways in each classification is included in **Appendix A** to this General Plan; this listing serves as the Circulation Plan for the County.

1) Principal Arterial System. The principal arterial system consists of a connected network of continuous routes having the following characteristics:

- Serve corridor movements having trip length and travel density characteristics indicative of substantial state wide or interstate travel.
- Serve all, or virtually all, urban areas of 50,000 and over population and a large majority of those with population of 25,000 and over.
- Provide an integrated network without stub connections except where unusual geographic or traffic flow conditions dictate otherwise, such as to connections to coastal cities and inter-governmental boundaries.

2) Minor Arterial System. The minor arterial road system should, in conjunction with the principal arterial system, form a network having the following characteristics:

- Link cities, and towns above 5,000 population (and other traffic generators, such as major recreational areas with equivalent capacity for generating and attracting travel over long distances) and form an integrated network providing interstate and intercounty service.
- Be spaced at such intervals, consistent with population density, so that all developed areas of the County are within a reasonable distance of an arterial.
- Constitute routes whose design should be expected to provide for relatively high overall travel speeds with minimum interference to through movement.



Selected References

3) Connectors.

- They provide connections between the higher order systems serving those corridors that have low-volume/long-trip length characteristics.
- Connectors provide service to any county seat not on a higher system and to the larger towns not directly served by the higher order systems.
- They provide service to mining, agricultural, or recreational areas having State or regional importance not served by the higher order systems.
- Connectors will be found more predominately in the very sparsely developed areas where there are relatively few local roads; e.g., in the desert areas, mountainous and heavily timbered portions of the State, and where there are large public land holdings.

4) Collector Road System. The collector routes generally serve travel of primarily regional importance rather than Statewide importance and constitute those routes on which (regardless of traffic volume) predominant travel distances are shorter than on arterial routes.

- Major Collector Roads
 - Provide service to the larger towns not directly served by the higher systems and to other traffic generators of equivalent intra-regional importance, such as consolidated schools, shipping points, county parks and important mining and agricultural areas.
 - Link the above places with nearby larger towns or cities, or with routes of higher classification.
 - Serve the more important intra-regional travel corridors.
- Minor Collector Roads.
 - Spaced at intervals, consistent with population density, to collect traffic from local connectors and local roads and bring all developed areas within a reasonable distance of a collector road.
 - Provide service to the remaining smaller communities.
 - Serve to link locally important traffic generators.

5) Local Connectors.

- Provides access to residential, commercial, and agricultural areas not served by a higher system.
- Provides access to local schools, recreational areas and other community facilities.
- Link the Local Road System with Collector Road System.

6) Local Roads.

- Serve primarily to provide access to adjacent land.
- Provide service to travel over relatively short distances as compared to collectors or other higher systems.
- Constitute the rural mileage not otherwise classified.

GENERAL PLAN; Pedestrian and Bicycle Systems Policies

As bicycles become more popular for both recreation and relatively short distance general trips, they must be considered with pedestrian infrastructure including the attendant impacts to roads, lanes, and sidewalks.

Policy DE-152: The County shall ensure that bicycle facilities are safe, attractive, and useful for both recreational and commuting cyclists. This shall include:

- Requiring that bicycle facilities be designed in accordance with the State Bikeway Design Criteria.
- Periodically reviewing, and updating if needed, street standards to accommodate bicycle lanes where indicated on the Bikeway Master Plan.
- Designing bridges, over passes, under passes, etc. to be compatible with bicycle travel.
- Considering bicycle safety when implementing improvements for automobile traffic operations.
- Provide an information/education program to encourage use of the system and to promote safe riding.

In many cases bicycles must share the right of way available with pedestrians in a rural community.

Policy DE-153: Provide pedestrian and bicycle ways along public roadway systems consistent with community area goals and policies and where sufficient right of way is available.

This policy will put improvements first into areas with higher use intensity so the money spent will serve the most persons. In a sense this is a prioritizing system statement.

Policy DE-156: Concentrate pedestrian improvements along school and transit routes, in areas of established pedestrian activity, and adjacent to sites serving senior citizen and/or persons with disabilities

This policy ties the need for pedestrian infrastructure in the public right of way to development that increases the use in the public right of way.

Policy DE-157: When development occurs, require installation of pedestrian and bicycle systems or, if infeasible, the payment of in-lieu fees to fund improvements to bicycle and pedestrian facilities.

COUNTY'S, ROAD STANDARDS

The following selections are definitions and requirements from the Road Standards that are applicable to this plan for relationship to pedestrian infrastructure affording path of travel in compliance with the federal Americans with Disabilities Act (ADA) and applicable requirements in the California Building Code (CBC). Commentary for how each part relates to this plan is in *italics*.

ROAD STANDARDS; Introduction; Common Terms and Definitions

The General Plan and the Road Standards are intended to be consistent in concept. "C" sections references made below regarding Caltrans TSN information are not instructive for relationship to this plan as they are incorporated conceptually by other definitions and requirements.

Functional Road Classification. The classification of a road segment according to its purpose. The County of Mendocino General Plan classifies and lists certain roads according to the following classifications: Minor Arterial, Connector, Major Collector, Minor Collector, and Local Connector. All county roads that are not listed are considered Local Roads. Caltrans classifies roads in accordance with the "Transportation System Network" (TSN) designations, which are published by Caltrans for all "on system" roads in California. All TSN roads that are not listed by Caltrans are considered Local Roads as well. These TSN designations are listed in C-3 & C-4 of this standard.

This is where the distinction is made that divides uses into rural and urban for whether there are requirements to provide pedestrian infrastructure such as curbs gutter sidewalks.

Rural Road. Rural Road Standards shall apply in all areas not designated for urban road standards. Also, Rural Connector Roads shall be used to connect isolated subdivisions and businesses with existing County public roads under the conditions described in Tab C, Road Design Standards.

The reference given to disabled persons use for sidewalks is important as sidewalks are the most prevalent feature and comprise the most length of any route used by disabled persons. Recognition that Owners can construct sidewalks gives another option for getting improvements put in place for public benefit.

Sidewalk. An improved path for public convenience that will not endanger persons or property and will accommodate the disabled. (S&H Sec. 907. Any owner or occupant of land may construct a sidewalk on the county highway along the line of his/her land, subject to the authority conferred by law on the Board of Supervisors.)

Selected References

This definition provides for requiring certain pedestrian infrastructure features in the road right of way by defining various characteristics existing, by defining geographic areas where standards apply, and by certain high intensity uses. The second portion gives some discretion to the "approval authority" to require urban style pedestrian infrastructure based on planning considerations affording pedestrian infrastructure when the project is built so it doesn't become an existing gap in the system when later developments fill in the surrounding area.

Urban Road. Urban road standards shall apply when any one of the following conditions are met:

- 1) Any of the property abutting the road has a General Plan land use classification with a base density of less than forty thousand square feet per parcel, regardless whether public water or sewers are available or currently extended to the property.
- 2) Urban improvements, such as curb, gutter, or sidewalk, are in existence on the road frontage of property adjacent to, or in the vicinity of, the development.
- 3) The road is within an urban area on a map prepared by the State of California Department of Transportation or the County of Mendocino Department of Transportation. As of February 2007, urban areas had been designated for Ukiah, Willits, and Fort Bragg on said maps.

Subject to the Approval Authority, urban road standards may apply when the road is within an urbanized or urbanizing area as evidenced by one or more of the following:

- 1) the road is within a city sphere of influence and pre-zoned by the city at a base density of less than one acre,
- 2) it is within the urban limit line within the Coastal zone,
- 3) sites with certain uses having high occupancies regardless of zoning. For example- multi-family housing, some civic uses, or other uses with high traffic volumes constituting urban land use intensities (over 15 people per acre).

ROAD STANDARDS; Road Design Standards (from tab C)

These statements are similar in nature to the General Plan Development Element goals for transportation systems and are more specific for describing functions relative to road systems uses.

C.1.C) Purpose and Intent. To ensure that roads, accessways and future road easements are provided to accommodate the vehicular and pedestrian traffic needs and related uses generated by development and use of the property, and the orderly development of the surrounding area. Roads must be designed so as not to cause or accelerate erosion that delivers sediment to water bodies while providing the intended access. Roads shall be designed for the following purposes:

- 1) Provide functional access to the properties that are intended to take access from the road.
- 2) Afford vehicular access to points of origin and destination.
- 3) Provide reasonably direct routes between points of origin and destination.
- 4) Provide for the orderly future development of the area.
- 5) Accommodate the amount of traffic projected to use the road.
- 6) Allow room for the passage and turning of the types of vehicles that will normally use the roads.
- 7) Provide room for the parking of vehicles where the intensity of abutting uses is likely to generate a need for such parking.
- 8) Provide facilities for walkways, bikeways, equestrian trails, buses, and other means of alternative transportation, when required.
- 9) Provide sufficient grade, cross slope, and/or facilities for the conveyance of storm water.
- 10) Storm water conveyance facilities within fish and aquatic life bearing jurisdictional streams shall, in addition to providing for storm flow, also meet any conditions set by appropriate agencies (California Department of Fish and Game, National Oceanic and Atmospheric Administration, etc.) for fish and aquatic life passage.
- 11) Accommodate the grade, turning, and passage needs of emergency and service vehicles.

- 12) Be free of structural defects.
- 13) Reasonably withstand wear.
- 14) Be easily and cost effectively maintained.
- 15) Integrate with and complement the natural terrain and vegetation, or be visually enhanced by the preservation and/or planting of appropriate trees and vegetation.
- 16) Not initiate or compound hydrologic or geologic problems.
- 17) Minimize surface area that cumulatively affects air quality, air temperature and storm water runoff.

This is a requirement statement derived from the urban road definition.

C.1.D) Urban Road Standards. Urban road standards shall apply when any one of the following conditions are met:

- 1) Any of the property abutting the road has a General Plan land use classification with a base density of less than forty thousand square feet per parcel regardless whether public water or sewers are available or currently extended to the property.
- 2) Urban improvements, such as curb, gutter, or sidewalk, are in existence on the road frontage of property adjacent to or in the vicinity of the development.
- 3) The road is within an urban area on a map prepared by the State of California Department of Transportation or the County of Mendocino Department of Transportation.

Subject to the Approval Authority, urban road standards may apply when the road is within an urbanized or urbanizing area as evidenced by one or more of the following: the road is within a city sphere of influence and pre-zoned by the city at a base density of less than one acre; it is within the urban limit line within the Coastal zone.

This is the requirement statement derived from the rural road definition.

C.1.E) Rural Road Standards. Rural Road Standards shall apply in all areas not designated for urban road standards. Rural Local Connector roads shall be used as the connector road when a new subdivision or business, open to the general public and requiring a county use permit, with urban roads is more than 2,500 feet from the connection to an existing public road.

County development patterns are different than cities, typically having discontinuities and developments not adjacent to each other. These local road types are usually dead end with the only right of way available for pedestrian infrastructure and have a higher importance for pedestrian route continuity.

C.1.F) Non Continuous Local Road Standards. Non Continuous Local Road Standards shall apply to both urban and rural settings depending on their existing use or planned use as set forth in County of Mendocino land use documents. Local Roads in Mendocino County will generally fit into the conditions described in the American Association of State Highway and Transportation Officials (AASHTO) "Very Low-Volume Local Roads – Under 400 vpd (vehicles per day) guidelines" with modifications as defined in these standards. Local Roads have some but not necessarily all of the following characteristics:

- Such roads would likely never be upgraded to interconnect to other parts of the countywide transportation system as Connectors, but may potentially serve developable tracts of land capable of supporting the specified volume of vehicles.
- The Local road designation does not preclude such roads from being interconnected to the countywide road system but does require that the proposed road is - by its location and/or length - not likely to become an alternative route for nonresident drivers as the AASHTO "Very Low-Volume Local Roads" guidelines are based on use by local drivers who learn the road.
- Such roads would likely never be upgraded and used to interconnect parts of the countywide transportation system. These standards shall provide for non-connected local road systems serving low population density developments and larger residential lot division projects, which result in a very low volume traffic, up to 400 vehicles per day.



Selected References

Road Design Standards (from tab C)

C.2. Definitions

These definitions below are included to show they are consistent and have been coordinated with the General Plan and how the functional classifications relate to the Caltrans system. This is important as Caltrans definitions are derived from the federal system and definitions many times are critical to funding a project. So if necessary, County general plan consistency can be verified and correlated to County road standards then to State Caltrans system then to the federal system.

C.2.A) *Functional Classification.* As defined and listed by the CALTRANS, Transportation System Network (TSN) can be viewed at: <http://dot.ca.gov/hq/tsip/hpms/Page1.php>.

C.2.B) *Principal Arterial.* As defined and shown in County of Mendocino General Plan. U.S. 101 is the only Principal Arterial listed in the General Plan. Because Caltrans Highway Design Standards apply to State Highways, County Road Standards have not been developed for Principal Arterial roads. Roads in this category also have a functional classification of *Urban or Rural Principal or other Arterial* (01&11-Principal Arterial Interstate-PIA, 12-Principal Arterial – Other Fwys or Expwys-OFE, & 02&14-Other Principal Arterial-OPA) as defined by CALTRANS, Transportation System Network (TSN).

C.2.C) *Minor Arterial.* As defined and shown in the County of Mendocino General Plan. Roads in this category also have a functional classification of *Urban or Rural Minor Arterial* (06&16-MA-Minor Arterial) as defined by CALTRANS, Transportation System Network (TSN).

C.2.D) *Connector.* As defined and shown in the County of Mendocino General Plan. Roads in this category are the same as Minor Arterials that have the functional classification of *Minor Arterials* (06&16-MA-Minor Arterial) as defined by CALTRANS, Transportation System Network (TSN).

C.2.E) *Major Collector.* As defined and shown in the County of Mendocino General Plan. Roads in this category also have a functional classification of *Major Collector* (07-MJCMajor Collector) as defined by CALTRANS, Transportation System Network (TSN).

C.2.F) *Minor Collector.* As defined and shown in the County of Mendocino General Plan. Roads in this category also have a functional classification of *Minor Collector or (Urban) Collector* (08-MNC or 17-Collector-COL) as defined by CALTRANS, Transportation System Network (TSN).

C.2.G) *Local Connector.* As defined and shown in the County of Mendocino General Plan. Roads in this category ARE THE SAME AS *Local roads - Rural & Urban* which have a functional classification of (09&19-LOC-Local) as defined by CALTRANS, Transportation System Network (TSN).

C.2.H) *Local Road.* A road is defined as a local road when it is not classified by the County of Mendocino General Plan as any of the above classifications. Roads in this category also have a functional classification of *Local roads - Rural & Urban* which have a functional classification of (LOC) as defined by CALTRANS, Transportation System Network (TSN).

Road Design Standards (from tab C); Standards

The C3 section is where requirements are stated that have broader application, typically to more than one road type or classification or to recurring situations requiring additional considerations for design.

This is where the connection is made to road shoulders used as pedestrian walks or sidewalks and requiring compliance with the ADA and CBC. Here we also find bicycles on the shoulder and discretion given to the "Approval Authority" for wider widths or additional walks based on public safety or convenience.

C.3.F) *Minimum Shoulder Width, Walkways, and Bikeways, Rural Roads.* Shoulders on rural roads above the local road classification shall be a minimum width of eight feet. Of that width, a minimum of five feet shall be paved to provide a walkway and bikeway. The Approval Authority may require additional walkways and/or bike paths for rural roads if necessary in the interest of public safety or convenience. The material used for rural walkways shall provide a smooth, even surface meeting the accessibility requirements of the Americans with Disabilities Act (ADA) and California Title 24. Asphalt concrete shall be considered as the standard material for rural walkways. Aggregate base, decomposed granite, and soil-cement will be considered as alternate materials for rural walkways, based on the recommendation of the DOT Director and subject to the Approval Authority.

Traditionally curb gutter and sidewalk is used. This standard requirement allows options and variations that suitable for different conditions that currently exist in remote communities and urbanizing rural communities.

C.3.P) *Curb, Gutter, and Walkways.* Curb, gutter, and walkways shall be required on all urban roads, and shall be constructed according to the Standard Road Plans. Walkways may be either monolithic with the curb or separate from it - hydrologically disconnected walkways and curbs leaving a vegetated or otherwise porous median for storm water filtration has been shown to be an effective practice. Curb and gutter on urban roads shall be Portland cement concrete. The material used for walkways shall provide a smooth, even surface meeting the accessibility requirements of the Americans with Disabilities Act (ADA) and California Title 24. Porous materials, as shown on MENDOT STD NO. A40, shall be encouraged for urban walkways. Appropriate porous materials, asphalt concrete, brick, interlocking pavers, and wood will be considered as alternate materials for urban walkways, based on the recommendation of the DOT Director and subject to the Approval Authority. Developers utilizing such alternate materials may be required to provide a supply of such alternate materials to DOT for use in future repairs.



Community Categories

Community Categories; CATEGORY 1

Category 1 is considered as the highest rank for potential development and population density in the near future. Examples are the urban density areas adjacent to the cities of Ukiah and Fort Bragg. It is expected that the County will put a significant amount of current development processing staff effort toward these areas based on anticipated highest activity level.

Characteristics are:

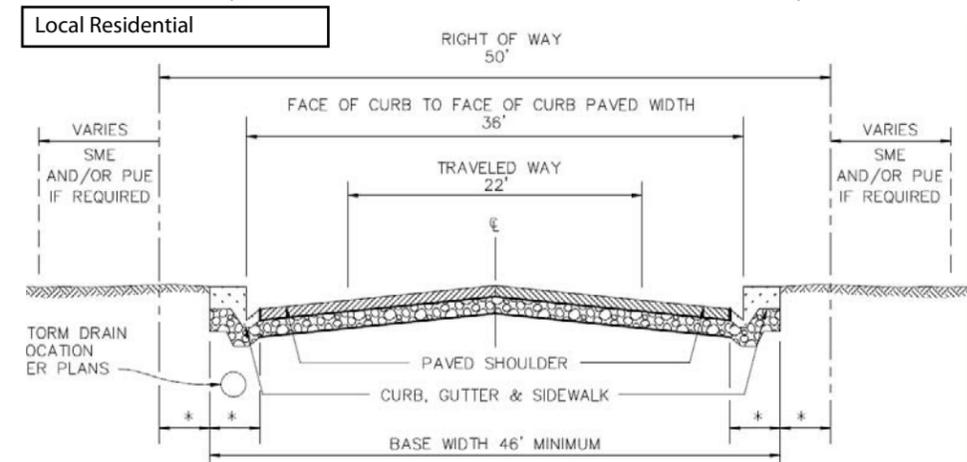
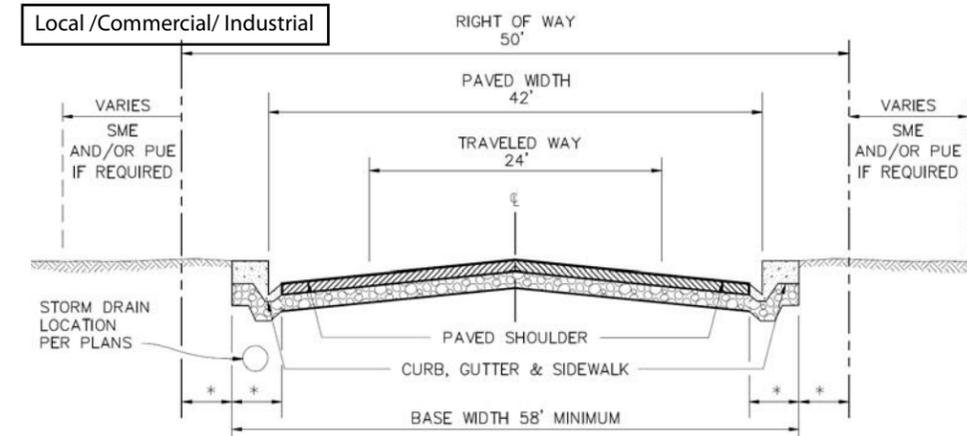
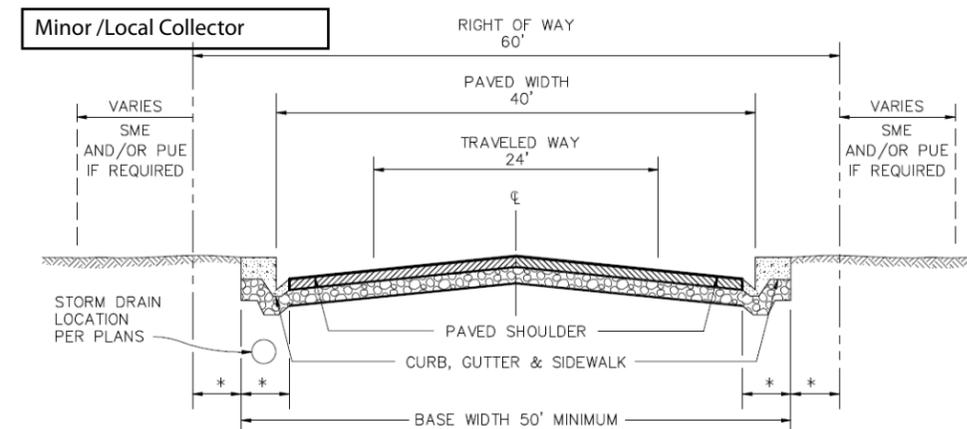
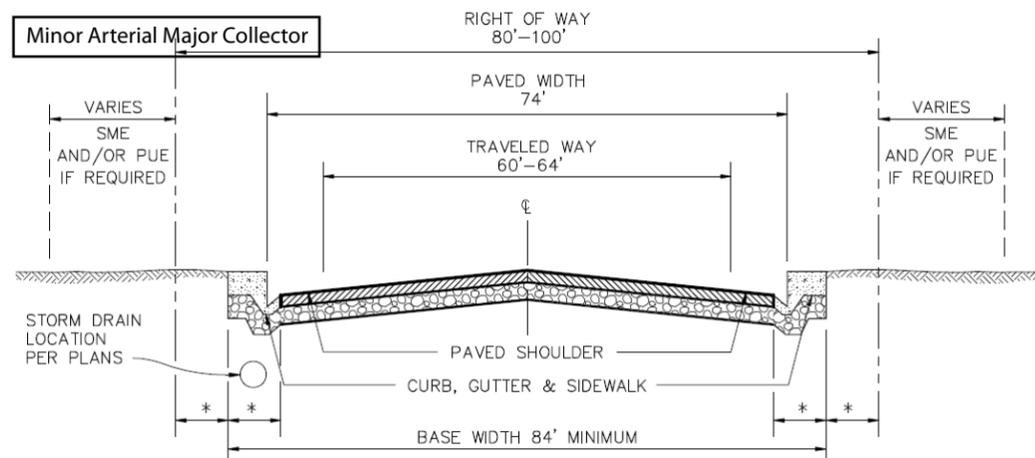
The community is adjacent to an existing city where the economic conditions coupled with land use standards are more favorable for development with the attendant influence from the urban or metro area for expanding or intensifying housing and businesses.

The community is within an urban planning area providing for the orderly development via ordinances, regulations, and standards. Development standards allow overall higher density via smaller parcel size for housing, higher density housing, and larger business types with higher use intensity.

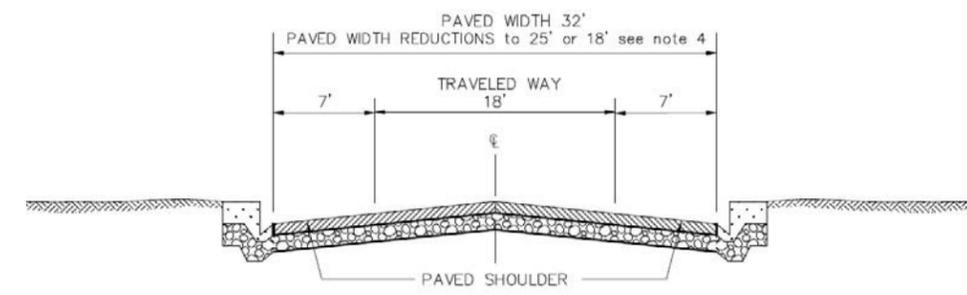
The community may be recognized as a separate entity even though near a larger urban or metro area but usually is associated as a part of the urban city.

The community can be identified as a travel destination or provides significant visitor serving facilities. One of the premiere economic factors for Mendocino County is tourism. There are areas that are by themselves a destination and others that support the traveling tourist by them intending to stay in Mendocino County or travel through to another destination. Use intensity increases as a county destination becomes more popular or regional area destinations become more popular causing a use increase at stopover locations. The influence for development is more toward business intensification since the customers are mainly tourists or visitors that rely on higher density transient housing versus the more land intense resident citizen housing.

Road types associated to the full spectrum urban environment are all inclusive from arterial to local with urban infrastructure standards required for road design. Existing roads most likely will be urban type with occasional local and rural types without urban features. See the road types to the right showing the design profiles and features required for urban development.



STANDARD



REDUCED

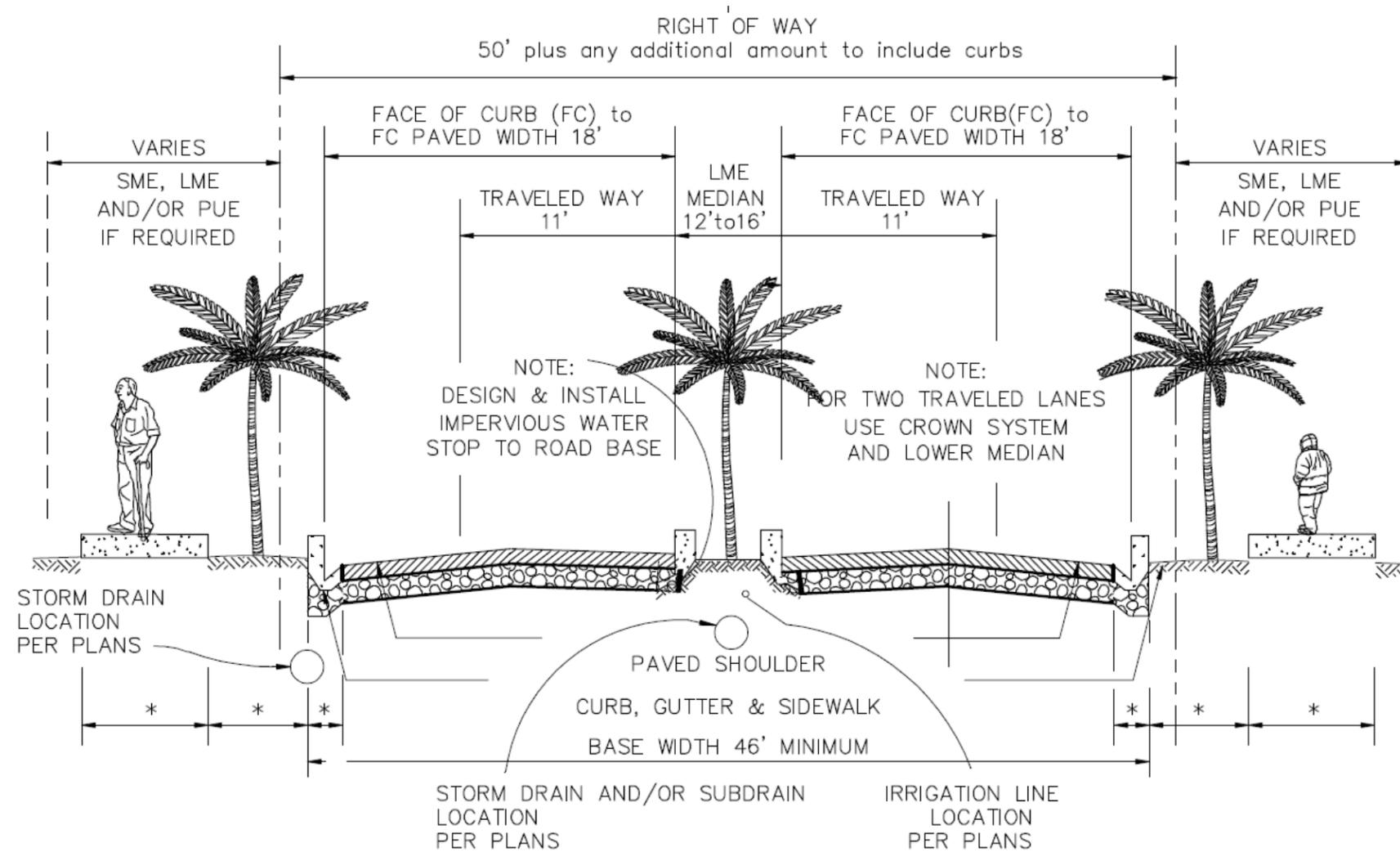
Details not shown same as standard



Community Categories

Community Categories; CATEGORY 1

Healthy Neighborhood Street Design



LEGEND:

	PAVEMENT
	BASE
PUE	PUBLIC UTILITY EASEMENT
SME	SLOPE MAINTENANCE EASEMENT
LME	LANDSCAPE MAINTENANCE EASEMENT. SEE NOTE 3.

* = VARIES, SEE NOTE 3.

NOTES:

1. PAVED SHOULDER TO SERVE AS PARKING LANE WHERE PARKING IS PERMITTED.
2. RIGHT OF WAY BEHIND SIDEWALK MAY BE USED FOR ROADSIDE FACILITIES SUCH AS SIGNS, PUBLIC UTILITIES, LANDSCAPING AND DRAINAGE FACILITIES.
3. SEE MENDOT STD. NO. A40 FOR CURB, GUTTER, SIDEWALK, & MEDIAN LANDSCAPE MAINTENANCE DETAILS.
4. SEE MENDOT STD. TAB(s) C.3.H & H CONCERNING WIDTH REDUCTIONS RELATED TO NO STREET PARKING.



Community Categories

Community Categories; CATEGORY 2

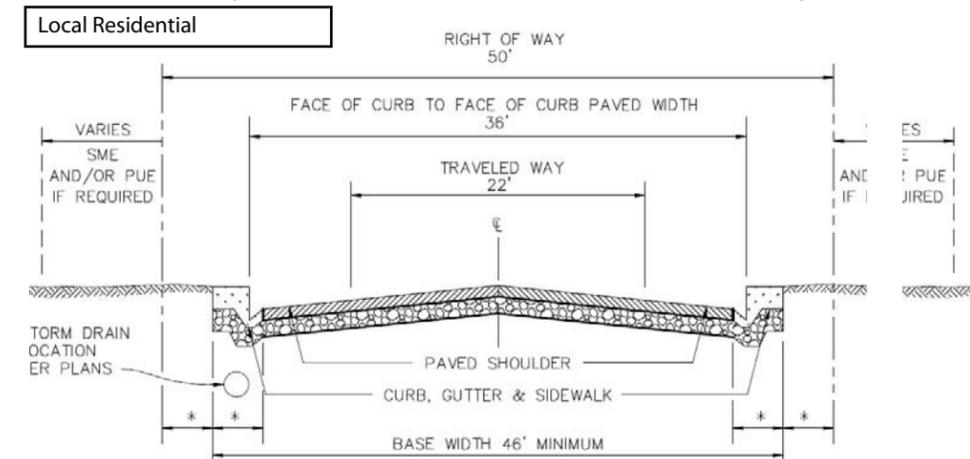
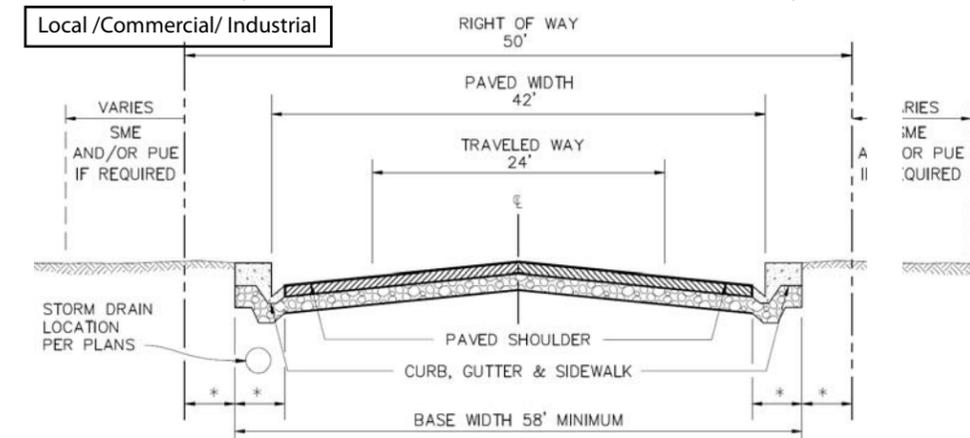
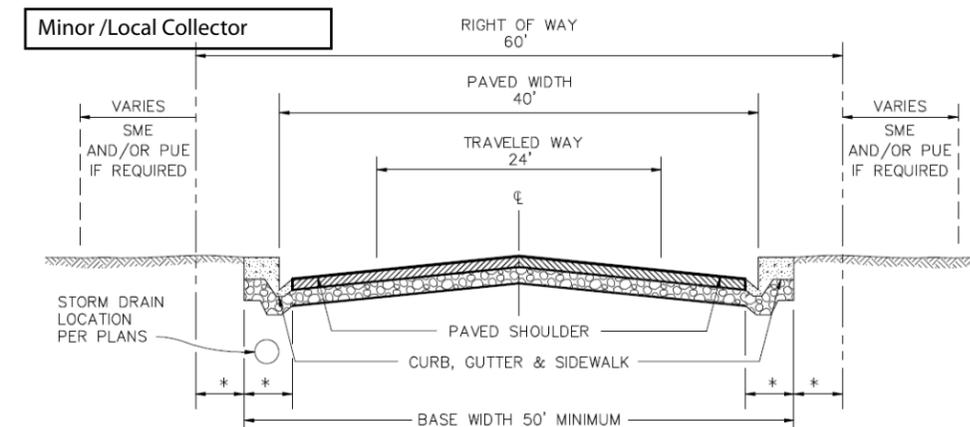
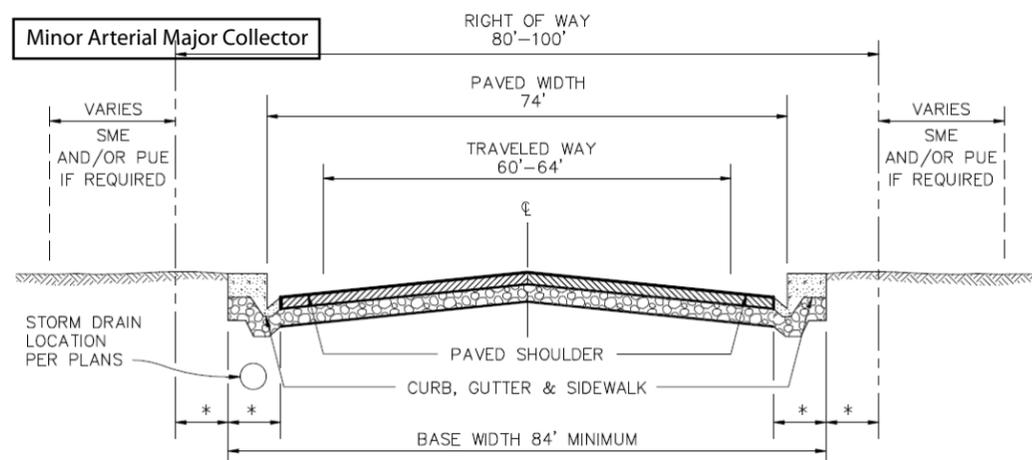
Category 2 comprises the areas near a city that will have relatively high potential for development and population density but to a lesser level than areas adjacent to a city. A good example is Talmage near Ukiah, there is an undeveloped rural separation from the current developed urban area to the Talmage town or village. It is expected the County will put measurable effort in development processing staff effort toward these areas in the near future.

Characteristics are:

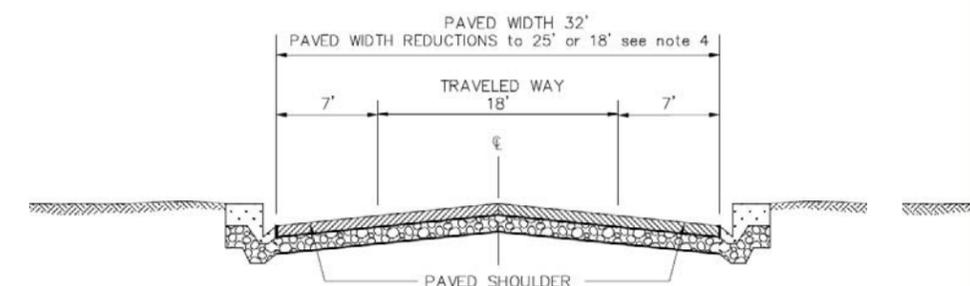
The community being considered is within an urban planning area, either fully adopted for orderly development or being developed in an identified urban area with an adoption in the near future. The economic conditions coupled with land use standards are more favorable for development with attendant influence to a lesser degree than being adjacent to an urban or metro area for expanding or intensifying housing and businesses. Development standards allow overall higher density via smaller parcel size for housing, higher density housing, and larger business types with higher use intensity. Potential for high density is present but not immediate for current development.

The community is recognized as a separate entity even though near a larger urban or metro area and is not associated at this time as a part of the urban city. Over time as development fills in and intensifies, this community area will most likely become a named neighborhood when absorbed by the nearby city.

Road types associated are the full spectrum urban environment and are all inclusive from arterial to local with urban infrastructure standards required for road design. Existing roads most likely will be a majority of local and rural types without urban features. See the road types to the right showing the design profiles and features required for urban development.



STANDARD



REDUCED

Details not shown same as standard



Community Categories

Community Categories; CATEGORY 3

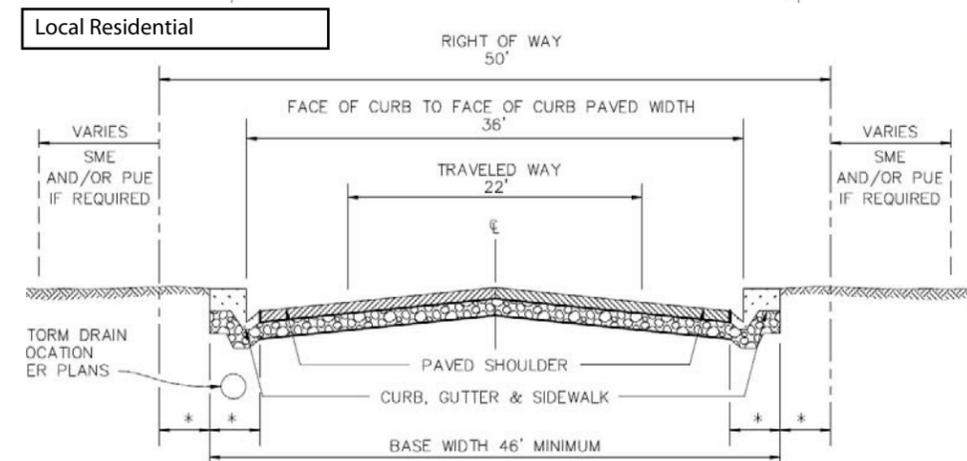
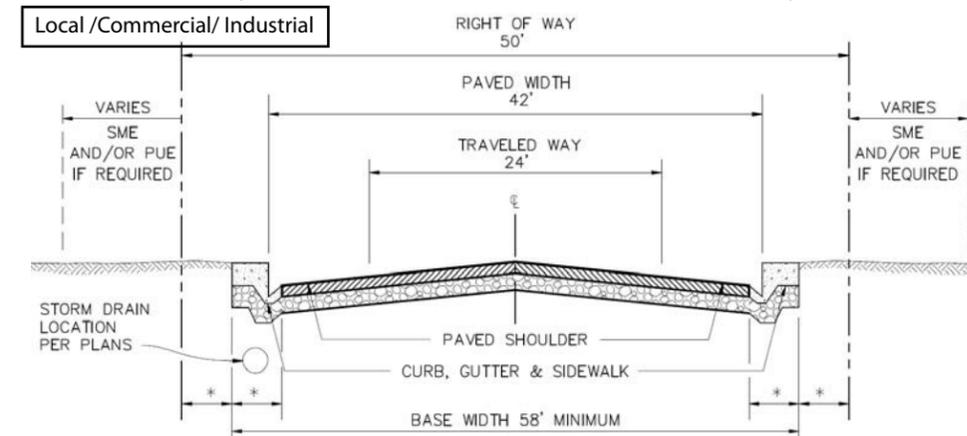
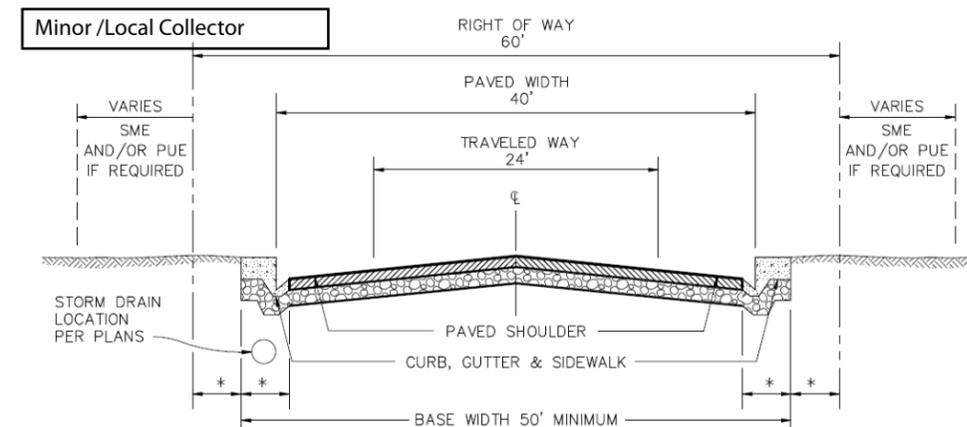
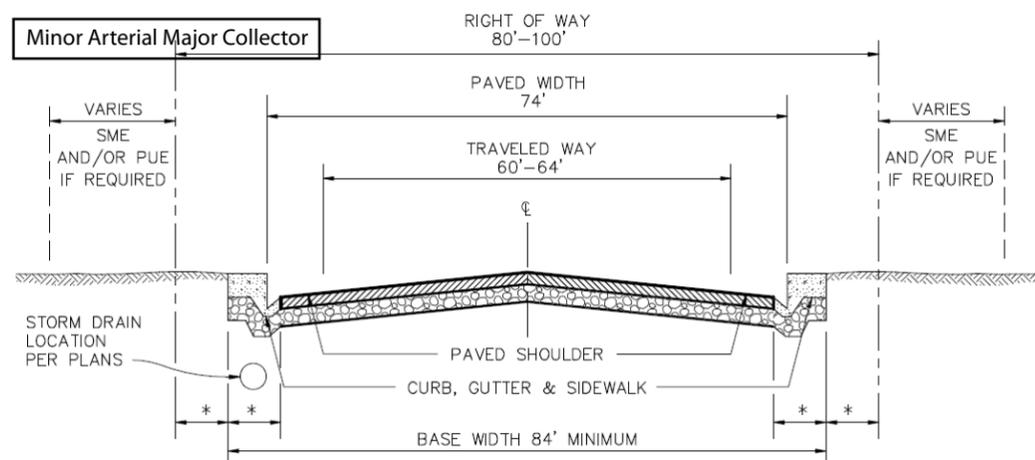
Category 3 communities are the stand alone towns and villages with enough distance from higher density urban cities that they can be considered on their local situation for development density. Examples are Boonville, and Town of Mendocino. It is expected the County will put measurable effort in development processing staff effort toward these areas as circumstances require.

Characteristics are:

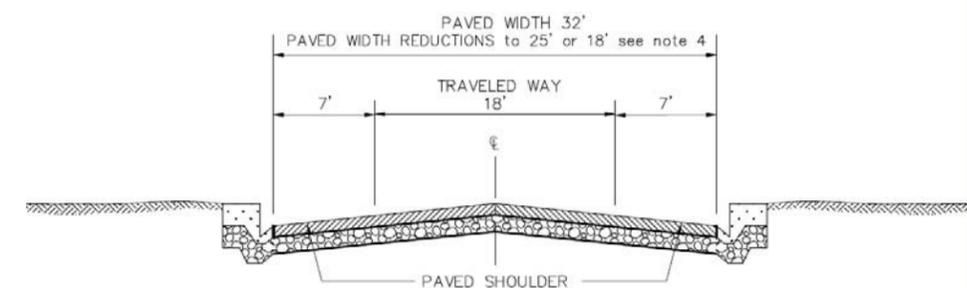
The community is very recognizable as a distinct separate town in a rural setting. Given the right circumstances, these towns can have significant pressure for higher density development. Influence to develop is typically constrained by local economy being self contained to sustain only current population or environmental limits such as water supply or restricted by legal requirements such as the Coastal Plan that limits development intensity. Development standards allow some higher density via smaller parcel size for housing, some medium density housing, and medium to small business types. Some community specific plans will allow larger and higher intensity use businesses. Potential for high density is sometimes present with difficult constraints.

The community can be identified as a travel destination or provides significant visitor serving facilities. One of the premiere economic factors for Mendocino County is tourism. There are areas that are by themselves a destination and others that support the traveling tourist either intending to stay in Mendocino County or travel through to another destination. Use intensity increases as a county destination becomes more popular or regional area destinations become more popular causing a use increase at stopover locations. The influence for development is more toward business intensification since the customers are mainly tourists or visitors that rely on higher density transient housing versus the more land intense resident citizen housing.

Road types associated with category 3 usually range from connector to local and sometimes have a minor arterial such as a state route highway on the town edge or the town situated each side on the arterial. Urban infrastructure standards are generally required for road design yet the existing road system is predominantly rural with some urban infrastructure installed. Urban infrastructure is generally a mix of different design types. See the road types to the right showing the design profiles and features required for urban development.



STANDARD



REDUCED

Details not shown same as standard



Community Categories

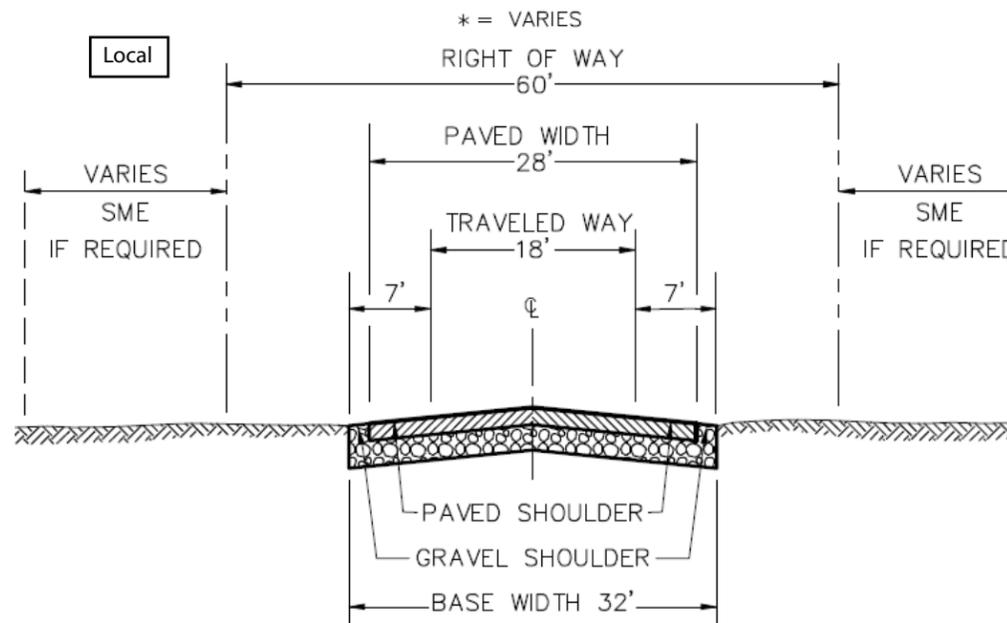
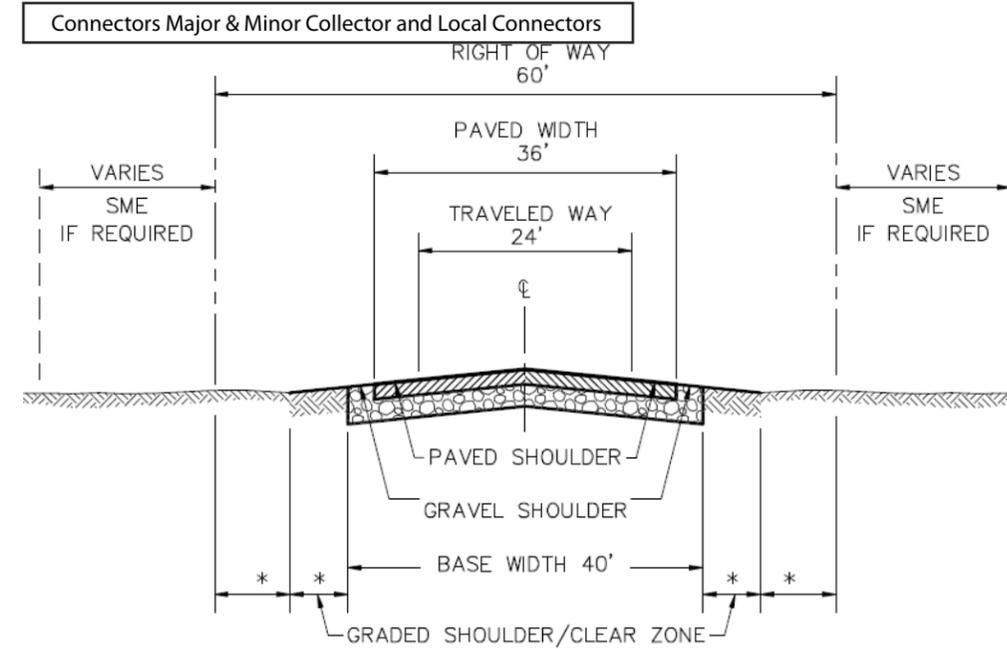
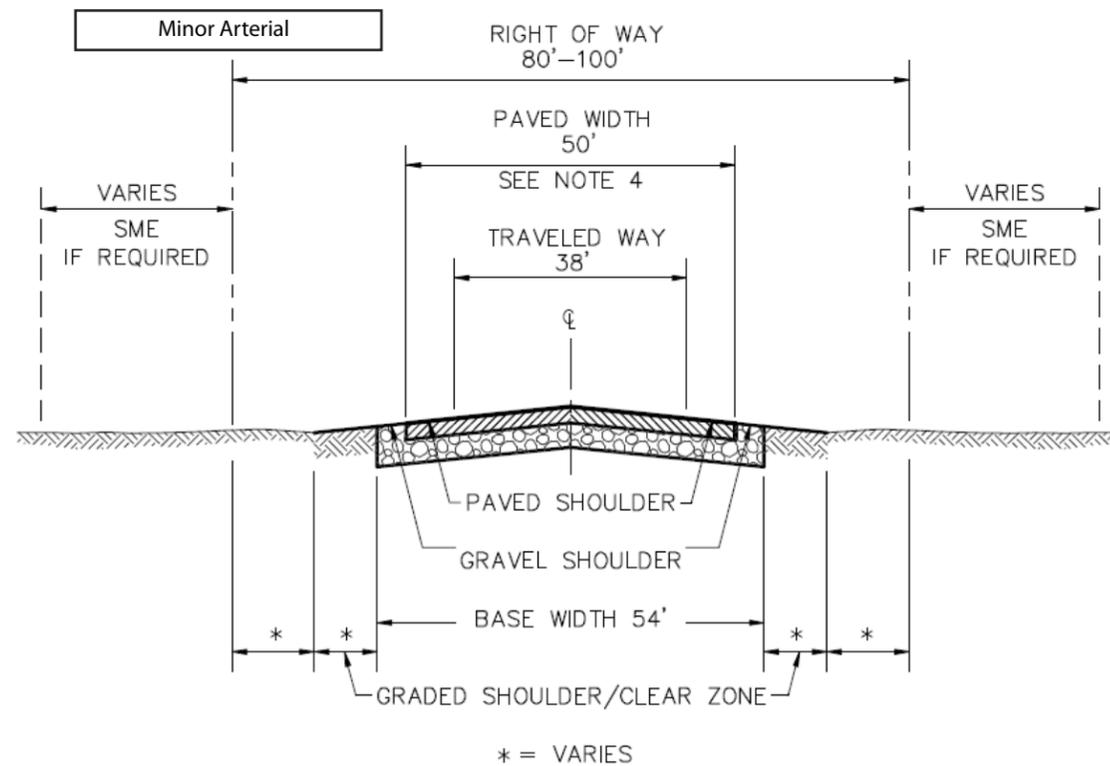
Community Categories; CATEGORY 4

Communities ranked category 4 are the rural density very small town areas with some being quite remotely located in relation to urban areas. Examples are Albion and Comptche. These rural settings are considered the lowest potential for development density due to economic or other constraints. It is expected the County will not need much development processing staff effort to serve these areas as development is anticipated to consist of primarily small residential projects and an occasional small commercial occupancy project.

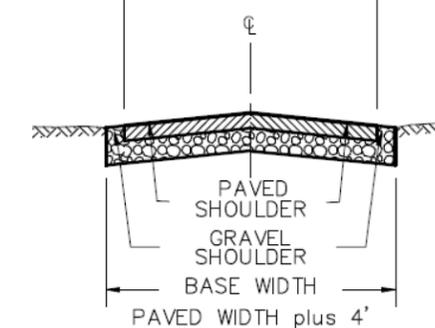
Characteristics are:

Recognizable as a distinct separate rural town even though there may be hardly any town or village for the named area. Typically the town is a couple of commercial or civic use buildings situated near the named post office with acreage size residential uses surrounding the core location. Due to remote location or environment constraints, there is very little development except single residences.

Existing road types are predominantly rural with no pedestrian infrastructure existing except the occasional wide paved shoulder. Future new development may require urban standard pedestrian infrastructure if the project use is commercial with urban land use intensity or generates high traffic volume. From a policy perspective, this community category will require some discretion and judgment to require urban road standards for right of way. See the road types to the right showing the design profiles and features required for rural development.



STANDARD



REDUCED



Albion

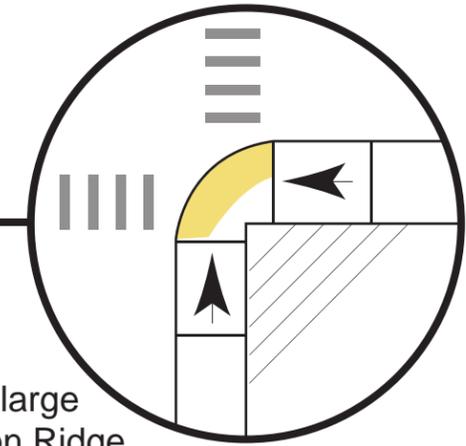
Overview

Category 4

This small community is rural density for considering road right of way development. The town area is primarily scattered housing on large parcels, a handful of homes on smaller parcels, and a couple of buildings comprising commercial and civic facilities just north of Albion Ridge Road.

Terrain in the area is predominately coastal bluffs with steep slopes. There are gentle to moderate on-site slopes at the store and post office. Generally the terrain is severe enough to consider pedestrian infrastructure technically infeasible in most cases. Currently there is disabled parking on-site at the post office and store.

Current land use zoning afford limited opportunities over time to develop more commercial and light industrial facilities in the recognized town area. Pedestrian infrastructure should only be considered in the right of way if on-site features cannot serve needs for pedestrians.



Mendocino
County

Location:

Albion

CalTrans Map & Year:

No Map

ADA
Comprehensive
Access Plan

Overview



SALLY SWANSON
ARCHITECTS, INC.
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490 POST STREET, SUITE 830
SAN FRANCISCO, CA 94102
WWW.SWANARCH.COM

Project #:
29075

Date:
05-28-10

Sheet:

Boonville

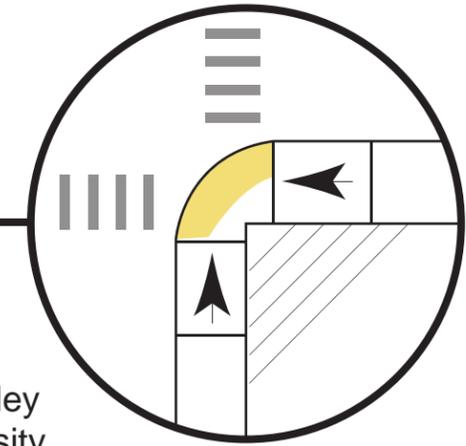
Overview

Category 3

This small rural town is generally urban density for considering road right of way development along Highway 128 from Anderson Valley Senior Center at southeast to Mountain View Road intersection to the northwest. Surrounding the commercial core is some tract density housing, large parcel housing estates, and agriculture. Local history and culture (boontling) coupled with numerous nearby vineyards make this town a popular tourist destination. There are a number of restaurants and retail businesses along Highway 128. Southerly and central to the town area is the Mendocino County Fairgrounds. Traffic can be intense and highly congested during peak travel season.

Terrain in the urban density area is predominately flat from Highway 253 intersection to Lambert Lane, then sloping gentle to moderate from Lambert Lane to Mountain View Road. Pedestrian infrastructure is a patchwork of different types ranging from brand new sidewalk frontage at a new restaurant building project just being completed to rough gravel at a number of locations. The patchwork types include very old concrete, asphalt of varying ages and conditions, and meandering road side paths on dirt or gravel. Pedestrian travel paths are achievable along Highway 128.

Current land use zoning will afford opportunities over time to develop more commercial and industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:

Boonville

CalTrans Map & Year:

3H22 1994

ADA
Comprehensive
Access Plan

Overview



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Project #:

29075

Date:

05-28-10

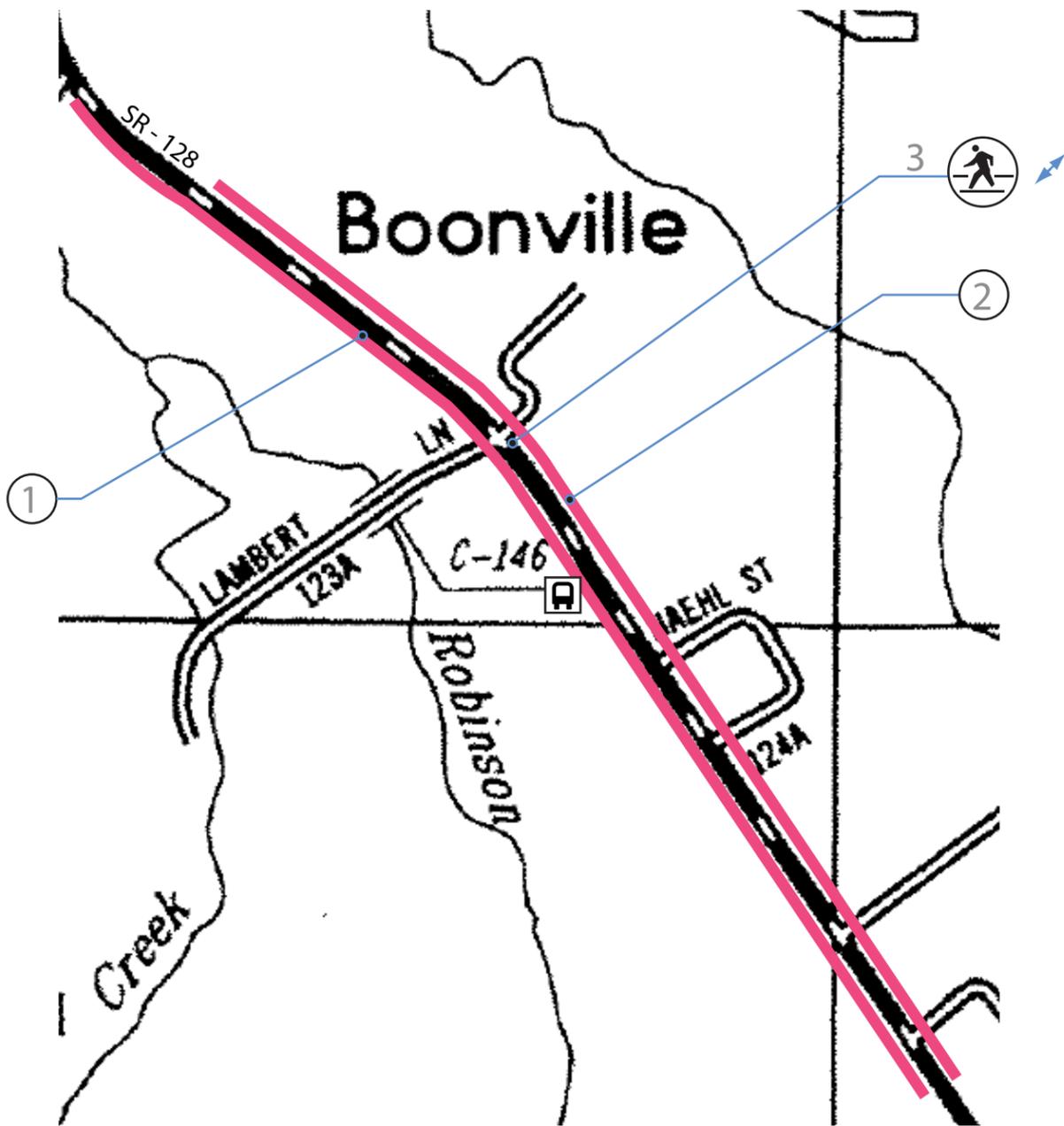
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Boonville

Category 3

Site Description:
An interesting local culture visitor destination rural town situated along Highway 128 with a well developed commercial urban density core along Highway 128.



Site Visit Pictures & Notes:

-  1: Sidewalk in this area is comprised of many different types and is generally not a continuous pedestrian system suitable for disabled access. Effort will be necessary to sort through the various conditions to improve this portion.
-  2: Sidewalk in this area is comprised of many different types and is generally not a continuous pedestrian system suitable for disabled access. Conditions for this portion involve very old walks and sidewalks that could be considered hazards, many curb ramps missing or not code compliant, and parking that severely interferes with developing a pedestrian walk system.
-  3: Crosswalk is recommended at this location due to distances between existing crossings. Markings, signage, and striping should be assessed on a warrant basis. Heavy bar striping with warning paddle signs is recommended for pedestrian safety crossing Highway 128.



Mendocino County

Location:
Boonville

CalTrans Map & Year:
3H22 1994

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Ref. North

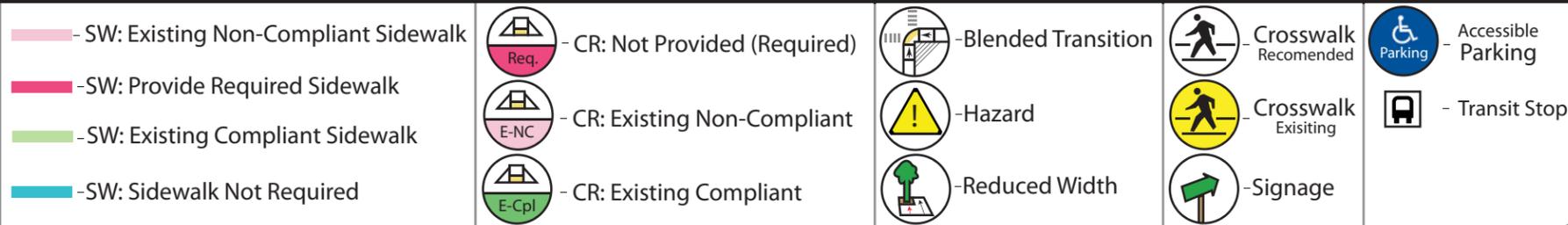
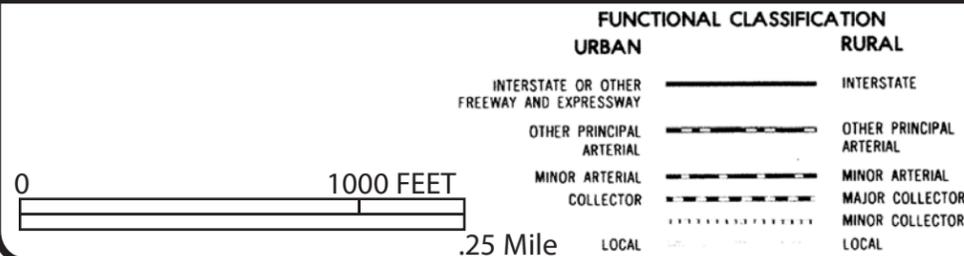


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Boonville Photographs: Items 1 - 3



Mendocino County

Location:

Boonville

ADA Comprehensive Access Plan

Photographs



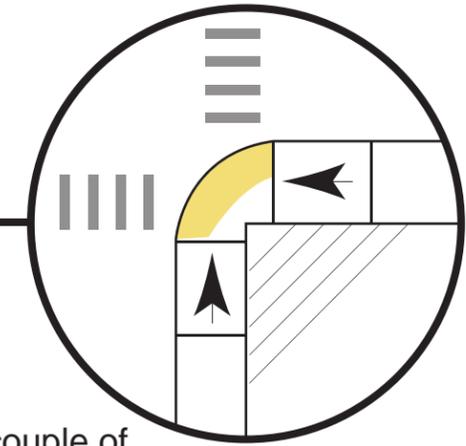
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Date: 06-30-10

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Brooktrails Township



Overview

Category 2

Brooktrails Township is recognizable as a distinct community very near the City of Willits and has a specific development plan for the Township area allowing urban density. The township area is primarily rural density housing on lots about one acre and larger, with a couple of commercial businesses on a collector street, and civic facilities clustered together. The only urban density observed is at the civic facilities and these buildings are situated in mountainous terrain making pedestrian infrastructure in the public right-of-way technically infeasible.

Terrain in the Township is predominately mountainside.

Current and future land use zoning will afford opportunities over time to develop more commercial and industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:
Brooktrails
Township

CalTrans Map & Year:
No Map

ADA
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Overview



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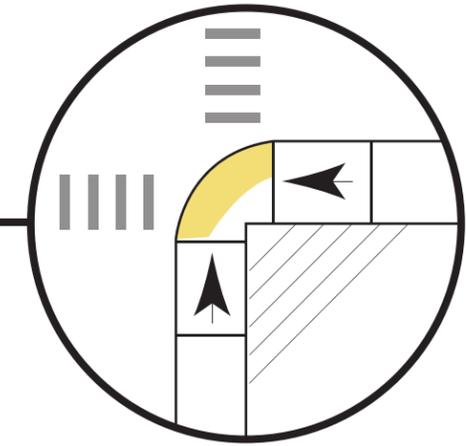
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Calpella

Overview

Category 2



This unincorporated town area is the northerly part of Ukiah “metro” area and also included as part of the County redevelopment plan area.. The town is a recognizable entity as it is a bit separated from north Ukiah. For development density, Calpella can be considered urban along North State Street from one block north of Moore Street to the southerly end of the apartment complex. The other urban density portion is along Moore Street from ½ block east of North State Street westbound to Facklam Court. Surrounding uses in the area are primarily residential with some tract density homes on Facklam Court.

Terrain in the urban density area is predominately gentle to moderate slopes with the area west of North State Street trending upward creating difficulty for pedestrian travel path along the right of way or on a site. An elementary school is located near the west terminus of Moore Street just east of US 101. Pedestrian travel paths are achievable in the flat and gentle sloped terrain but will be quite difficult uphill on Moore Street to the school.

This community is within the Ukiah Valley Plan currently being developed jointly by the City of Ukiah and County of Mendocino. Current and future land use zoning will afford opportunities over time to develop more commercial and industrial facilities in the town area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:

Calpella

CalTrans Map & Year:

3G53 1994

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Overview



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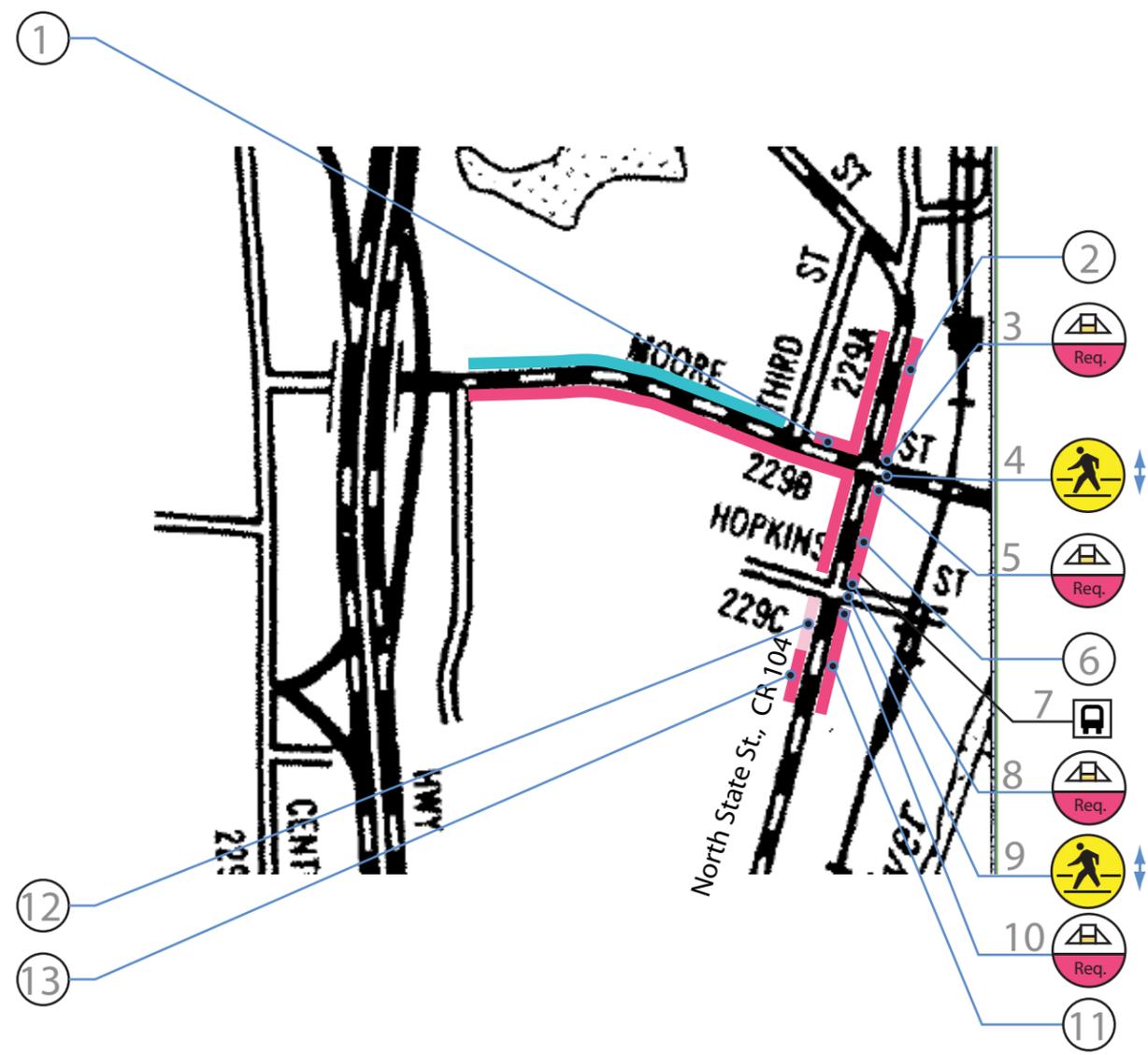
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Calpella

Category 2

Site Description:
A rural community within an urban planning area that has an urban density core at the intersection of North State Street and Moore Street.



Site Visit Pictures & Notes:

- 1: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. There are no destination facilities north of the Post Office. Surface slopes are nearly suitable for blended transition at street corner for pedestrian crossings.
- 2: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. There are no destination facilities north of the Laundromat. Surface slopes are nearly suitable for blended transition at street corner for pedestrian crossings.
- 3: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 4: Considering the business activity and weekend intensive traffic due to recreation destinations nearby, assessment on a warrant basis is recommended for pedestrian safety to determine markings and striping at pedestrian this crossing. Paving is quite rough in this intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.
- 5: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 6: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. Surface slopes are nearly suitable for blended transition at street corner for pedestrian crossings.
- 7: Bus stop; paving is deteriorating and should be repaired for sure footing. Bus pad and curb should be considered by the transit agency.
- 8: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 9: Crosswalk should be considered at this location due to traffic and children crossing during school times; this recommended crosswalk should be assessed on a warrant basis for safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
- 10: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 11: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. There are no commercial destinations south of the lumber yard.
- 12: Sidewalk; excessive cross slopes at driveways.
- 13: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. There are no commercial destinations south of the lumber yard.



Mendocino County

Location:
Calpella

CalTrans Map & Year:
3G53 1994

ADA
Comprehensive
Access Plan



Project #:
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Date:
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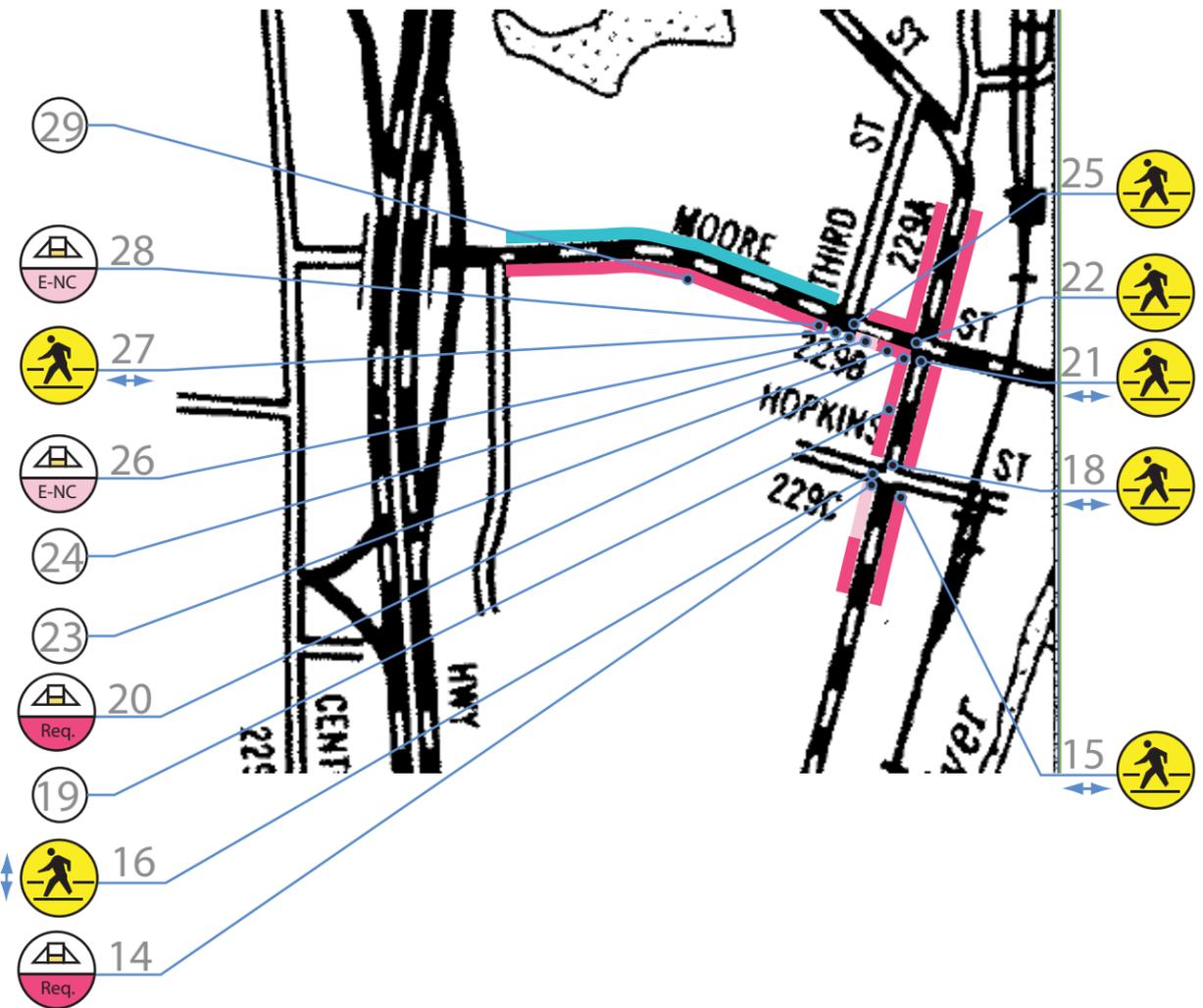
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INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY	—————	INTERSTATE	—————	INTERSTATE	—————
OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————
MINOR ARTERIAL	—————	MINOR ARTERIAL	—————	MINOR ARTERIAL	—————
COLLECTOR	—————	MAJOR COLLECTOR	—————	MAJOR COLLECTOR	—————
LOCAL	—————	MINOR COLLECTOR	—————	MINOR COLLECTOR	—————
LOCAL	—————	LOCAL	—————	LOCAL	—————

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
- SW: Sidewalk Not Required				

Calpella

Category 2

Site Description:
A rural community within an urban planning area that has an urban density core at the intersection of North State Street and Moore Street.



Site Visit Pictures & Notes:

- 14: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 15 & 16: Crosswalk should be considered at this location due to traffic and children crossing during school times; this recommended crosswalk should be assessed on a warrant basis for safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
- 17: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 18: Crosswalk; recommend striping for pedestrian safety. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
- 19: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well.
- 20: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 21 & 22: Crosswalk; considering the business activity and weekend intensive traffic due to recreation destinations nearby, there should be a crosswalk for pedestrian safety, heavy bar striping is recommended with high visibility sign paddles. Paving is quite rough in this intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.
- 23: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder until the concrete sidewalk near Facklam Court. Options may include asphalt if properly sloped with provisions for drainage, driveway access to properties, and connections to concrete sidewalks. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. s intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.
- 24: Sidewalk; excessive cross slope at driveways.
- 25: Crosswalk; recommend striping for pedestrian safety with high visibility paddle signs.
- 26: Curb ramp; undersized top landing, no detectable warnings, excessive ramp slope, lip at ramp bottom to gutter, excessive gutter slope.
- 27: Crosswalk; recommend striping for pedestrian safety.
- 28: Curb ramp; undersized top landing, no detectable warnings, excessive ramp slope, lip at ramp bottom to gutter, excessive gutter slope.
- 29: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder from the concrete sidewalk near Facklam Court. Options may include asphalt if properly sloped with provisions for drainage, driveway access to properties, and connections to concrete sidewalks. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions should be considered as well.



Mendocino County

Location:
Calpella

CalTrans Map & Year:
3G53 1994

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Comprehensive
Access Plan

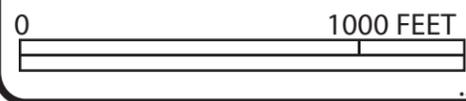


Project #:
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Date:
05-28-10

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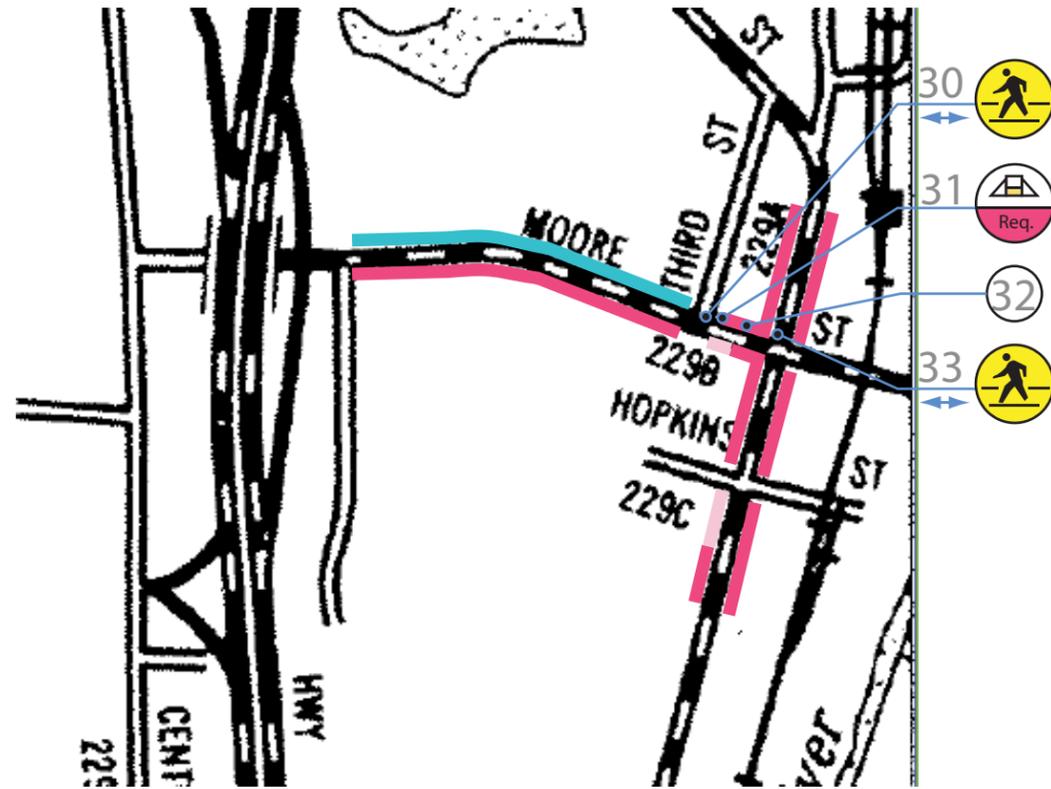
FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Calpella

Category 2

Site Description:
A rural community within an urban planning area that has an urban density core at the intersection of North State Street and Moore Street.



Site Visit Pictures & Notes:

-  30: Crosswalk; recommend striping for pedestrian safety.
-  15 & 16: Crosswalk; recommend striping for pedestrian safety. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
-  17: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  18: Crosswalk; recommend striping for pedestrian safety. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
-  19: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well.
-  20: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  21 & 22: Crosswalk; considering the business activity and weekend intensive traffic due to recreation destinations nearby, there should be a crosswalk for pedestrian safety, heavy bar striping is recommended with high visibility sign paddles. Paving is quite rough in this intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.
-  23: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder until the concrete sidewalk near Facklam Court. Options may include asphalt if properly sloped with provisions for drainage, driveway access to properties, and connections to concrete sidewalks. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. s intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.
-  24: Sidewalk; excessive cross slope at driveways
-  25: Crosswalk; recommend striping for pedestrian safety with high visibility paddle signs.
-  26: Curb ramp; undersized top landing, no detectable warnings, excessive ramp slope, lip at ramp bottom to gutter, excessive gutter slope.
-  27: Crosswalk; recommend striping for pedestrian safety.
-  28: Curb ramp; undersized top landing, no detectable warnings, excessive ramp slope, lip at ramp bottom to gutter, excessive gutter slope.
-  29: Sidewalk is not continuous in this area with the only pedestrian route available being along the road shoulder from the concrete sidewalk near Facklam Court. Options may include asphalt if properly sloped with provisions for drainage, driveway access to properties, and connections to concrete sidewalks. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions should be considered as well.



Mendocino County

Location:
Calpella

CalTrans Map & Year:
3G53 1994

ADA
Comprehensive
Access Plan



Project #:
29075

Date:
05-28-10

Sheet:

	FUNCTIONAL CLASSIFICATION		- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
	URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL	RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL				

Calpella Photographs: Items 1 - 30



Mendocino County

Location:

Calpella

ADA Comprehensive Access Plan

Photographs



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Calpella Photographs: Items 31 - 33



Mendocino
County

Location:

Calpella

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Caspar

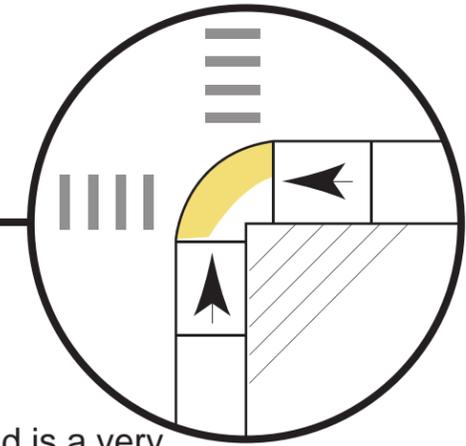
Overview

Category 3

This small coastal community is a recognizable rural town comprised of scattered residences surrounding a very small urban density commercial core village. The village area is attractive early 20th century style architecture. The town is situated west of Highway 1 and is a very short commute distance to Mendocino and Fort Bragg. Traffic is very light and appears to be just local residents and occasional business patrons to the few remaining businesses in the town core.

Terrain in the urban density area is predominately flat to gentle slopes. Current pedestrian infrastructure is the road shoulder that is not continuously paved. Pedestrian infrastructure improvements are easily achievable in the core village area.

Current land use zoning restricts opportunities to develop more commercial buildings or housing, and limits development intensity. There will be but a handful of construction development projects over time, mostly comprised of small new buildings and alterations or additions to existing. Pedestrian travel paths serving new or altered facilities should be done with the projects as they develop.



Mendocino
County

Location:

Caspar

CalTrans Map & Year:

2G1 1994

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Overview



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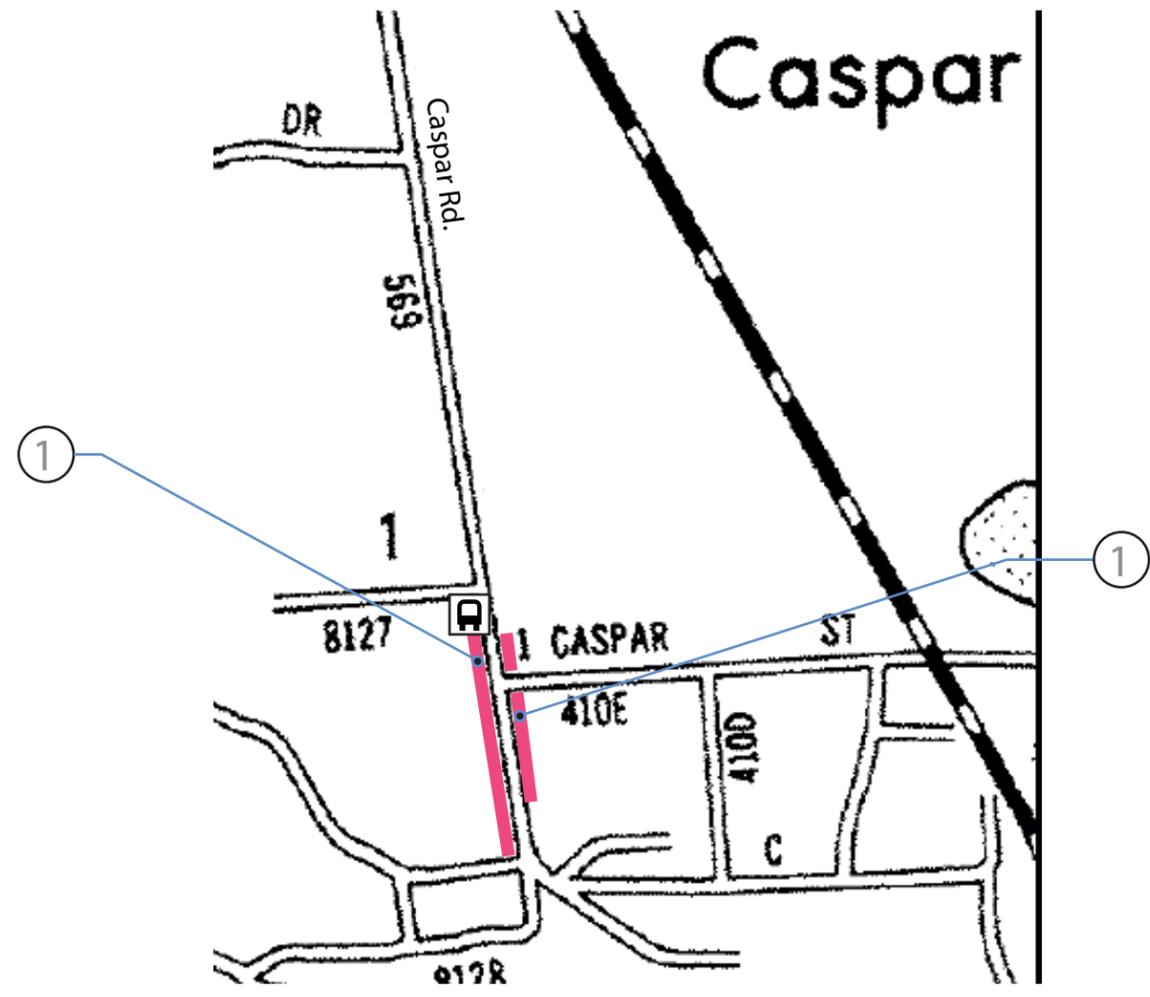
Caspar

Category 3

Site Description:
A small coastal town situated a short distance west of Highway 1 with a very small village commercial urban density core area.

Site Visit Pictures & Notes:

 1: Currently the available pedestrian walking path is along the asphalt road shoulder. Design for pedestrian improvements must consider parked car interferences, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage.



Mendocino County

Location:
Caspar

CalTrans Map & Year:
2G1 1994

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Access Plan



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Date:
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	FUNCTIONAL CLASSIFICATION		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
	URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL	RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL					

Caspar: Item 1



Mendocino County

Location:

Caspar

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Photographs



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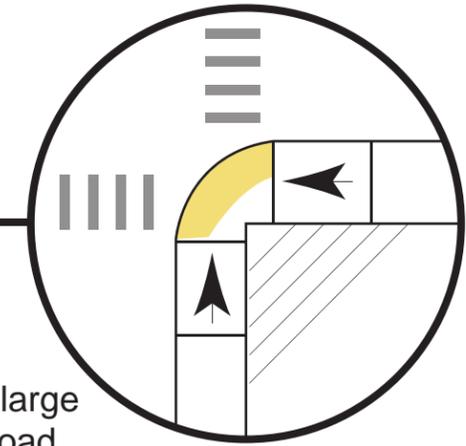
Overview

Category 4

This small community is rural density for considering road right of way development. The town area is primarily scattered housing on large parcels with a couple of buildings comprising commercial and civic facilities at the intersection of Comptche Road and Flynn Creek Road.

Terrain in the area is predominately low mountain and mountain side slope. There are gentle to moderate slopes around the intersection near the store and post office. Generally the terrain is severe enough to consider pedestrian infrastructure technically infeasible in most cases. The only recommendation for infrastructure is to provide for disabled parking with travel path connection serving the post office.

Current land use zoning will afford opportunities over time to develop more commercial and light industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:
Comptche

CalTrans Map & Year:
No Map

ADA
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Access Plan

Overview



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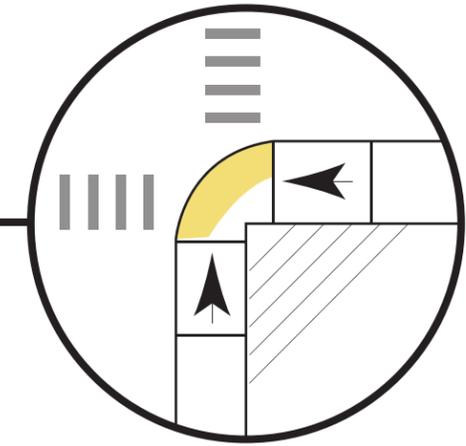
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Covelo

Overview

Category 3



This farming based community is remotely located about one hour from Willits on Highway 162 and is very self-contained for businesses, services, and civic facilities. The town is generally urban density for considering road right of way development. Surrounding the town area is large parcel agriculture with an Indian reservation just north. Two blocks west and two blocks south of Howard Street and Highway 162 are developed with traditional curb gutter and sidewalks that are deteriorating and near end of useful service life.

Terrain in the urban density area is predominately flat. Pedestrian travel paths are achievable at most locations in the town. The highest pedestrian use areas in town are the commercial and civic locations that are along each side of Highway 162 and two blocks west of 162 on Howard Street.

Commercial businesses have not fared well over time with many commercial buildings vacant and deteriorating. The economy base in this area is now primarily farming, with the return of lumber or a different industry the area could support more retail and service businesses. Current land use zoning will afford opportunities over time to develop and redevelop more commercial and industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:

Covelo

CalTrans Map & Year:

3F33 1994

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Overview



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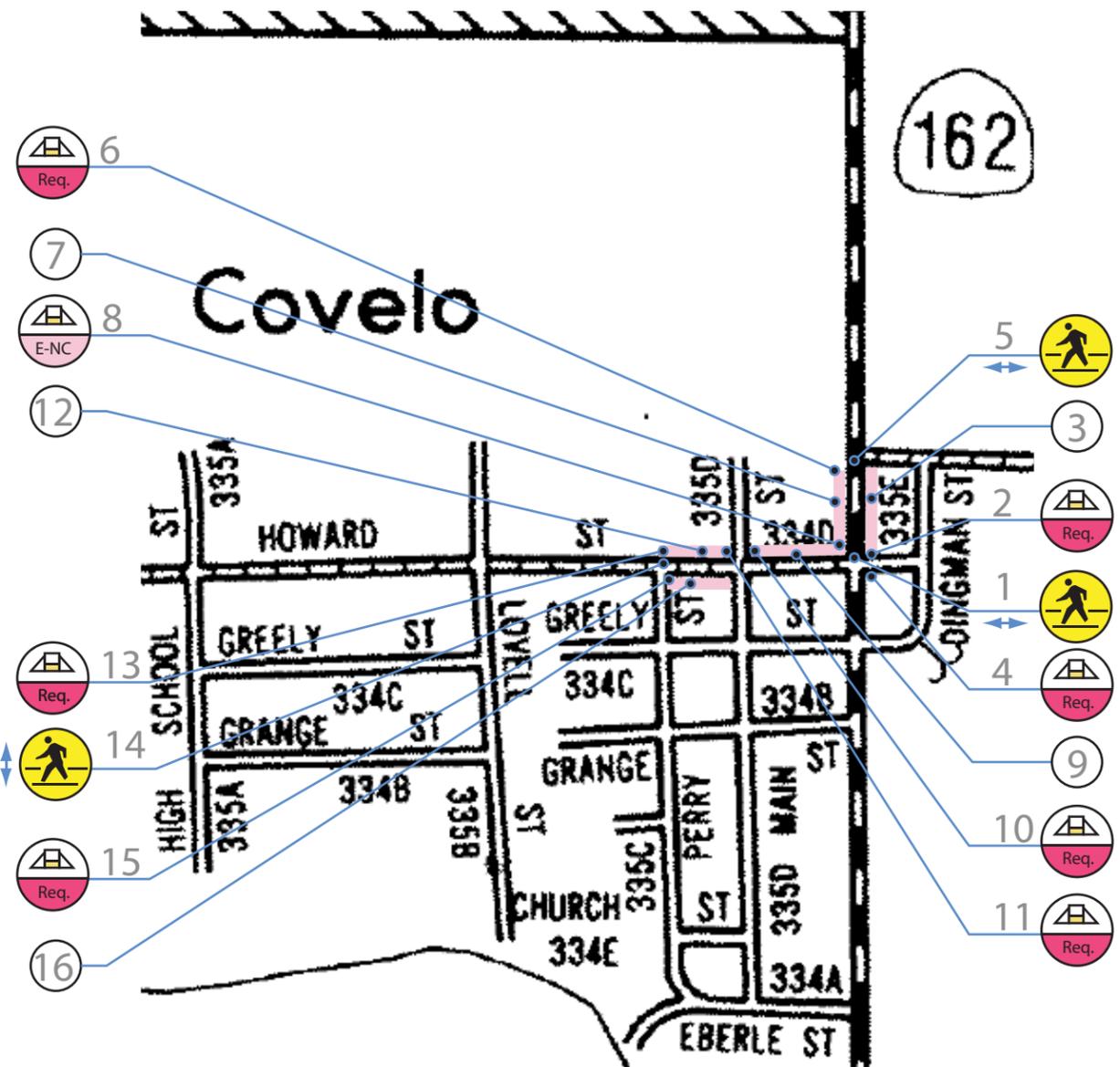
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Covelo

Category 3

Site Description:
A remote self contained rural community situated in a farming valley near an Indian reservation, the town has a small commercial urban density area along Highway 162 with some civic and commercial facilities on Moore Street.



Site Visit Pictures & Notes:

- 1: Crosswalk; rough pavement and ledges near corners, paving correction necessary.
- 2: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 3: Sidewalk; excessive cross slope, ledges, and discontinuities. Travel path also includes asphalt adjacent to parking areas without clear designation for walk area. Asphalt may surface may be able to be developed into a pedestrian path if properly sloped. Surface water drainage needs to be accommodated in any future work for pedestrian walks. Striping recommended on asphalt to identify vehicle drives and walk lanes. Parking restrictions or designated vehicle parking lanes should be considered as well.
- 4: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 5: Crosswalk; recommend striping for pedestrian safety, due to highway traffic, heavy bar stripes recommended. Paving is quite rough near corners and road edges and needs repair for pedestrian safety and disabled access compliance.
- 6: Curb ramp or blended transition is missing at this "tee" intersection and is required to provide pedestrian travel path to Highway 162 west side.
- 7: Sidewalk is not continuous in this area. Pedestrian routes available are along the road shoulder and adjacent to buildings on older non compliant concrete sidewalks. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well.
- 8: Existing curb ramp; very rough gutter area, excessive slopes at top, excessive run slopes due to road crown edges, no detectable warnings, striping doesn't include min 4ft maneuvering clearance at diagonal ramp bottom.
- 9: Sidewalk is not continuous in this area, portions are on gravel road shoulder. Existing concrete has rough surfaces, broken sections, ledges, and constricted width at direction change. Parking restrictions or designated parking lane should be considered.
- 10 & 11: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 12: Existing sidewalk has very rough portions, excessive cross slopes, and ledges.
- 13: Curb ramp or blended transition is missing at this "tee" intersection and is required to provide pedestrian travel path across Howard Street.
- 14: Crosswalk; recommend striping for pedestrian safety. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
- 15: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
- 16: Sidewalk is not continuous in this area, portions are on gravel and on road shoulder. Existing concrete has rough surfaces, broken sections, and ledges. Parking on street appears to have sufficient width.



Mendocino County

Location:
Covelo

CalTrans Map & Year:
3F33 1994

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Comprehensive
Access Plan

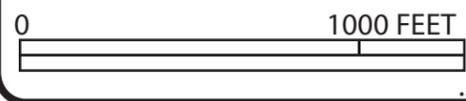


Project #:
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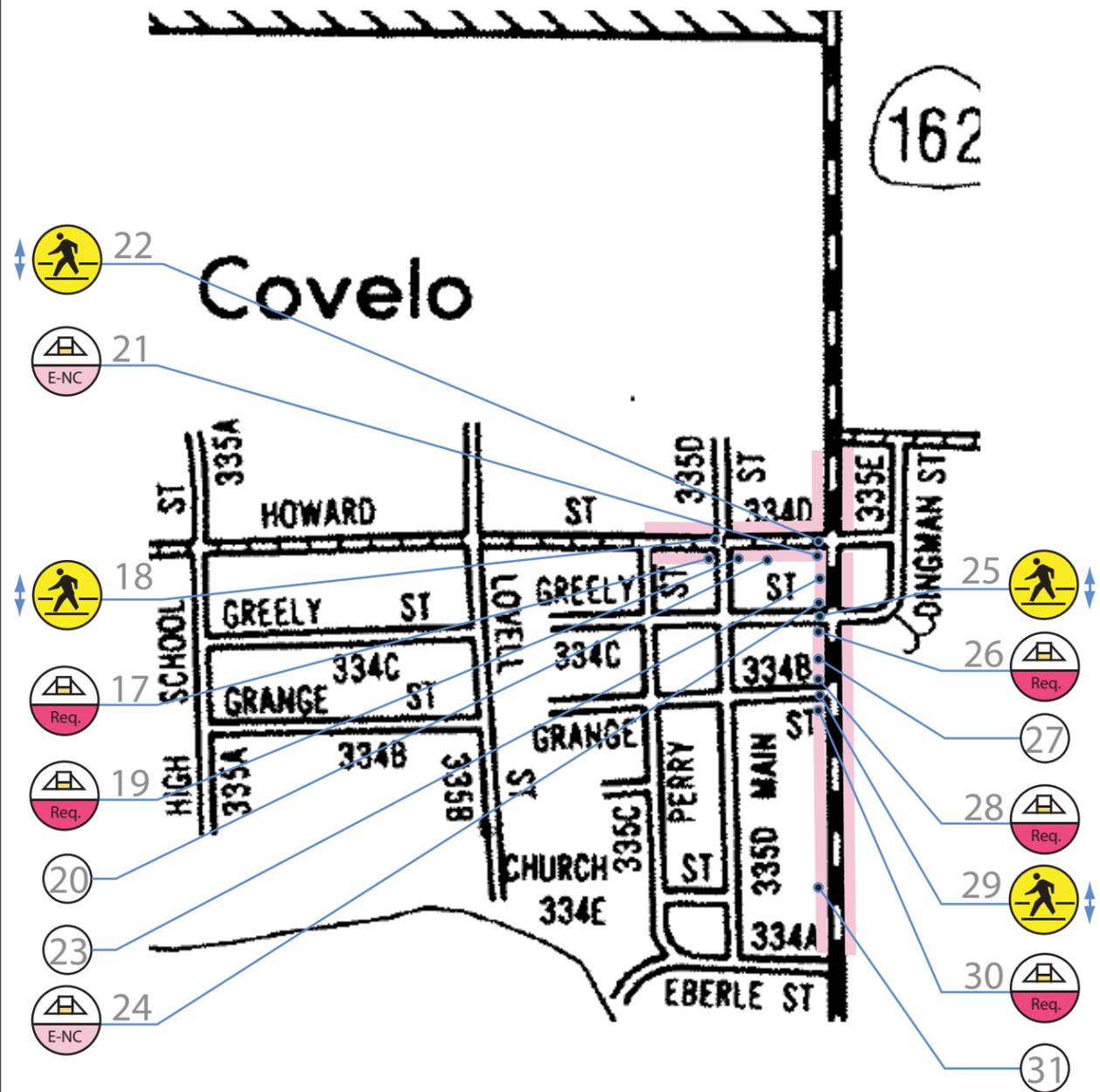
<p>FUNCTIONAL CLASSIFICATION</p> <p>URBAN</p> <p>INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY</p> <p>OTHER PRINCIPAL ARTERIAL</p> <p>MINOR ARTERIAL COLLECTOR</p> <p>LOCAL</p>		<p>RURAL</p> <p>INTERSTATE</p> <p>OTHER PRINCIPAL ARTERIAL</p> <p>MINOR ARTERIAL MAJOR COLLECTOR</p> <p>MINOR COLLECTOR LOCAL</p>		<p> - SW: Existing Non-Compliant Sidewalk</p> <p> - SW: Provide Required Sidewalk</p> <p> - SW: Existing Compliant Sidewalk</p> <p> - SW: Sidewalk Not Required</p>	<p> - CR: Not Provided (Required)</p> <p> - CR: Existing Non-Compliant</p> <p> - CR: Existing Compliant</p>	<p> - Blended Transition</p> <p> - Hazard</p> <p> - Reduced Width</p>	<p> - Crosswalk Recommended</p> <p> - Crosswalk Existing</p> <p> - Signage</p>	<p> - Accessible Parking</p> <p> - Transit Stop</p>
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Covelo

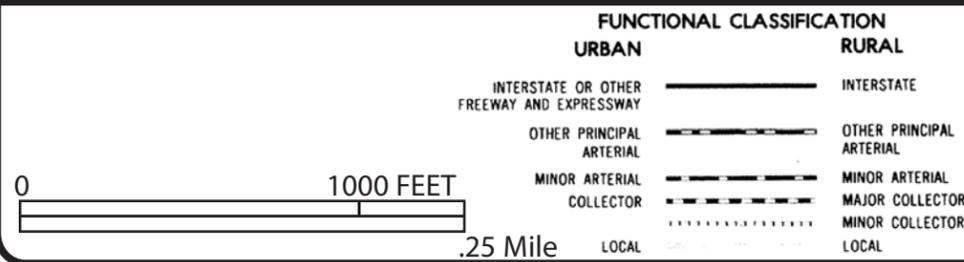
Category 3

Site Description:
A remote self contained rural community situated in a farming valley near an Indian reservation, the town has a small commercial urban density area along Highway 162 with some civic and commercial facilities on Moore Street.



Site Visit Pictures & Notes:

-  17: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  18: Crosswalk; recommend striping for pedestrian safety. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
-  19: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  20: Sidewalk; existing concrete has rough surfaces, broken sections, and ledges. A majority of the travel path is on asphalt walks that are in deteriorated condition. Parking on street appears to have sufficient width.
-  21: This corner has conditions that will allow a blended transition. Existing concrete has ledges and uneven surfaces requiring repairs. Detectable warnings are missing.
-  22: Crosswalk; rough pavement and ledges near corners, excessive run slope due to road crown; paving correction necessary.
-  23: Sidewalk; concrete is in poor condition; repairs necessary to provide pedestrian and disabled access compliant surfaces.
-  24: Curb ramp, blended transition; no detectable warnings, gutter area quite rough.
-  25: Crosswalk; recommend striping for pedestrian safety. Paving is quite rough and needs repair for pedestrian safety and disabled access compliance.
-  26: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  27: Sidewalk is not continuous in this area. Pedestrian routes available are along the road shoulder and adjacent to buildings on older non compliant concrete sidewalks. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well.
-  28: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  29: Crosswalk; recommend striping for pedestrian safety.
-  30: Curb ramp or blended transition is missing at this corner and is required to provide pedestrian travel path.
-  31: Sidewalk is not continuous in this area. Pedestrian routes available are along the road shoulder and adjacent to buildings on older non compliant concrete sidewalks. Options may include asphalt if properly sloped with provisions for drainage and driveway access to properties. Durable striping to delineate the walk portion on pavement should also be provided. Parking restrictions or designated vehicle parking lanes should be considered as well. There are no active businesses beyond the gas station.



 - SW: Existing Non-Compliant Sidewalk	 - CR: Not Provided (Required)	 - Blended Transition	 - Crosswalk Recommended	 - Accessible Parking
 - SW: Provide Required Sidewalk	 - CR: Existing Non-Compliant	 - Hazard	 - Crosswalk Existing	 - Transit Stop
 - SW: Existing Compliant Sidewalk	 - CR: Existing Compliant	 - Reduced Width	 - Signage	
 - SW: Sidewalk Not Required				



Mendocino County

Location:
Covelo

CalTrans Map & Year:
3F33 1994

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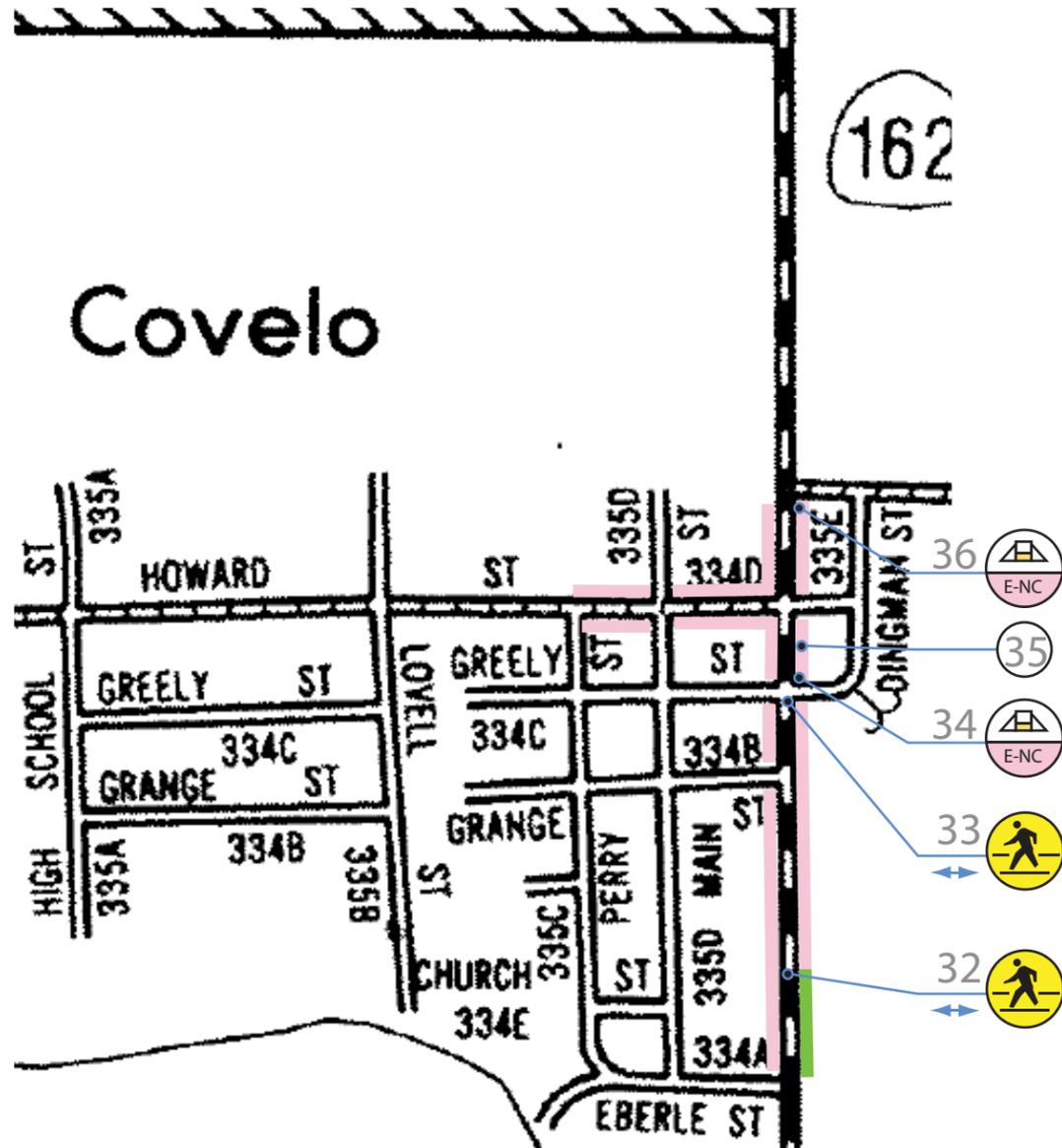
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Covelo

Category 3

Site Description:
A remote self contained rural community situated in a farming valley near an Indian reservation, the town has a small commercial urban density area along Highway 162 with some civic and commercial facilities on Moore Street.



Site Visit Pictures & Notes:

- 32: Crosswalk should be provided for connecting pedestrian paths; recommend striping for pedestrian safety, due to highway traffic, heavy bar stripes recommended. Paving is quite rough near road edges and needs repair for pedestrian safety and disabled access compliance.
- 33: Crosswalk; recommend striping for pedestrian safety, due to highway traffic, heavy bar stripes recommended. Paving is quite rough near corners and road edges and needs repair for pedestrian safety and disabled access compliance.
- 34: Curb ramp, blended transition; no detectable warnings, gutter area quite rough.
- 35: Sidewalk; concrete is in poor condition; repairs necessary to provide pedestrian and disabled access compliant surfaces.
- 36: Curb ramp, blended transition; no detectable warnings, gutter area quite rough.



Mendocino County

Location:

Covelo

CalTrans Map & Year:

3F33 1994

ADA Comprehensive Access Plan



Ref. North

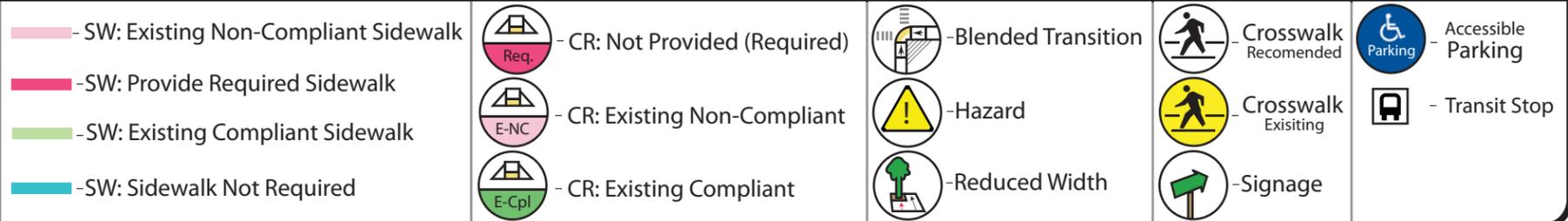
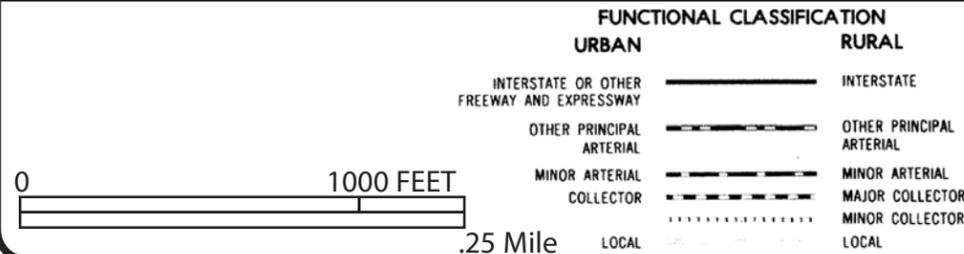


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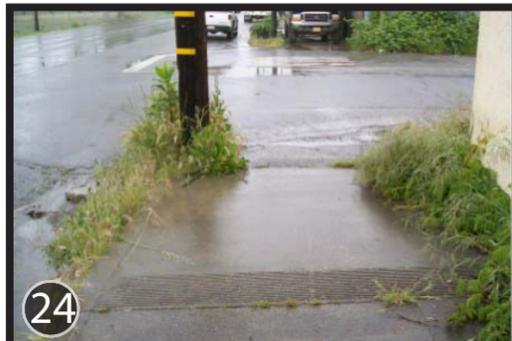
Project #: 29075

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Covelo Photographs: Items 1 - 27



Mendocino County

Location:

Covelo

ADA Comprehensive Access Plan

Photographs



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Covelo Photographs: Items 28 - 36



Mendocino County

Location:

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Elk

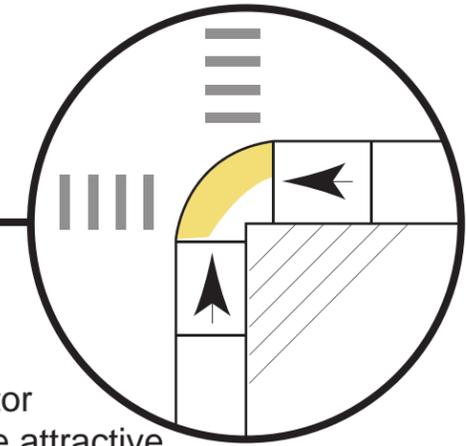
Overview

Category 3

This small coastal community is a recognizable rural town with a cluster of residences, commercial buildings, civic buildings, and visitor serving facilities that constitute urban density. The village area is unique early 20th century style architecture with most buildings quite attractive and very well maintained. The village is situated each side on Highway 1. This village is a known travel destination and during peak tourism periods has intense traffic on Highway 1.

Terrain in the urban density area is predominately gentle to moderate slopes with some parts of Highway 1 east side trending gently upward. Current pedestrian infrastructure is the paved asphalt road shoulder that has constricted width from road lane edge to paving edge in the village northerly area east side. Pedestrian travel paths are easily achievable along Highway 1.

Current land use zoning restricts opportunities to develop more commercial buildings and limits development intensity. There will be only a handful of construction development projects over time, mostly comprised of small new buildings and alterations or additions to existing. Pedestrian travel paths serving new or altered facilities should be done with the projects as they develop.



Mendocino
County

Location:

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CalTrans Map & Year:

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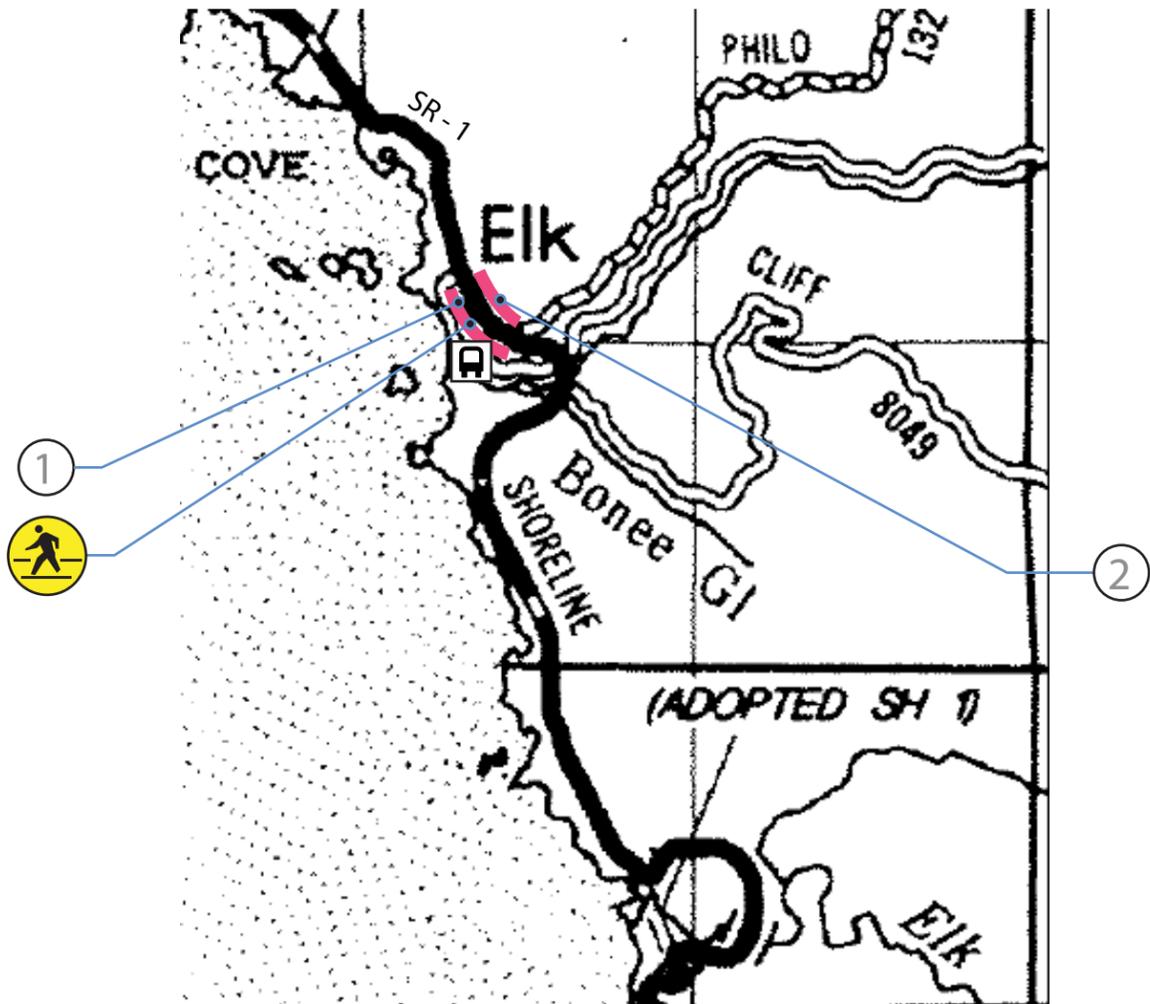
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41

Elk

Category 3

Site Description:
A travel destination coastal town, situated along Highway 1, with a very small village commercial area.



Site Visit Pictures & Notes:



1: Currently the available pedestrian walking path is along the asphalt road shoulder. Due to intense traffic, it is recommended to have a design that separates pedestrians from traffic lanes such as parking lane between walkway and road lane. Design for pedestrian improvements must consider how the pedestrian travel walkway will serve general travel along Highway 1, parked car interferences, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage.



2: Currently the available pedestrian walking path is along the asphalt road shoulder and the village northern area is very narrow from road lane edge to paving edge. Due to intense traffic, it is recommended to have a design that separates pedestrians from traffic lanes such as parking lane between walkway and road lane. Design for pedestrian improvements must consider grading to create additional road shoulder width along Highway 1, parked car interferences, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage.



Mendocino County

Location:

Elk

CalTrans Map & Year:

2H 1994

ADA Comprehensive Access Plan



Ref. North



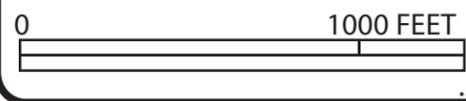
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FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		[Pink line] - SW: Existing Non-Compliant Sidewalk [Red line] - SW: Provide Required Sidewalk [Green line] - SW: Existing Compliant Sidewalk [Blue line] - SW: Sidewalk Not Required	[CR Not Provided] - CR: Not Provided (Required) [CR Existing Non-Compliant] - CR: Existing Non-Compliant [CR Existing Compliant] - CR: Existing Compliant	[Blended Transition] - Blended Transition [Hazard] - Hazard [Reduced Width] - Reduced Width	[Crosswalk Recommended] - Crosswalk Recommended [Crosswalk Existing] - Crosswalk Existing [Signage] - Signage	[Accessible Parking] - Accessible Parking [Transit Stop] - Transit Stop
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Elk Photographs: Items 1 - 2



Mendocino County

Location:

Elk

ADA Comprehensive Access Plan

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Sheet:

Fort Bragg

Overview

Category 1

Mendocino County has jurisdiction for a relatively large developing area surrounding the City of Fort Bragg to the north, south, and east. The land areas adjacent to the city have distinct location characteristics that are best addressed separately for unique urban density issues.

North Fort Bragg

Locally known as the “Pudding Creek” area; the mapped urban limit is a few hundred feet north from Pudding Creek confluence with the Pacific Ocean. There is no urban density outside the city limits and the county area northerly along Highway 1 has only a few widely spaced commercial businesses with a few residences on acreage size parcels interspersed. Areas east from Highway 1 and north of Pudding Creek are large acreage residential. This area has limited opportunity to intensify as a travel destination due to local coastal plan restrictions preserving the environment. Development for general commercial uses has not reached the perceived allowed density level resulting in some medium size business opportunities being available. Pedestrian infrastructure improvements for travel paths serving new facilities should be done with the projects as they develop.

East Fort Bragg

Adjacent to the east side city limits and southerly of Pudding Creek; the area is best characterized as residential approaching urban density. It appears the newer tract density housing developments observed in the area are inside the city limits most likely annexed prior to subdivision construction. The current development pattern for this area appears driven by utility infrastructure needs provided by annexation to the City. Pedestrian travel paths serving new facilities should be done with the projects as they develop and be coordinated with the City for connections.

South Fort Bragg, South Harbor Drive

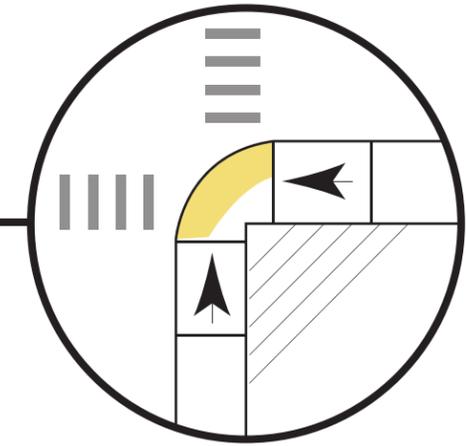
This is the “working” port area operated by the Harbor District. The locale is comprised of urban density industrial and marine businesses, a US Coast Guard patrol boat station, and road connections to marine related businesses at the port vicinity then easterly along Noyo River to private marinas. Since the Harbor District is a governing agency having elected or appointed officials sitting in a governance position and there are employees managing the District, the County should have the Harbor District construct any improvements within the District’s port operations area. Coordination will be necessary to plan any pedestrian infrastructure improvements in the port operations area. Other land portions adjacent to port operations appear best served by on site facilities for disabled persons as the uses are varied and more destination in character than a collection of similar uses.

South Fort Bragg, around and westerly from College of the Redwoods campus

The neighborhood is primarily larger lot residential, usually one acre in size, and just reaching urban density. This smaller peninsular area is sparsely developed with empty lots comprising the majority of land and many lots are currently offered for sale. There are no commercial facilities west of Highway 1 serving residents in this neighborhood, pedestrian improvements throughout the area are not warranted at this time. Pedestrian infrastructure improvements for travel paths serving new facilities should be done with the projects as they develop.

South Fort Bragg along Highway 20

Within the urban area defined by maps, development is very low density at this time. There are only a few widely spaced businesses and a few large acreage residences interspersed in the area. Pedestrian infrastructure improvements for travel paths serving new facilities should be done with the projects as they develop.



South Fort Bragg along Highway 1

Urban area limit defined by maps show the Hare Creek Bridge as the southern edge. Businesses along Highway 1 south of the bridge are widely spaced and on large parcels and this area is not considered urban density for pedestrian infrastructure improvements.

South Fort Bragg, North Harbor Drive

Locally known as “Noyo”; being adjacent to Noyo Bay on the Pacific Ocean and the connection into the harbor area via Noyo River. This is a commercial area with urban density and is a known visitor serving destination for restaurants having nice river or ocean views, marine sport businesses, fishing industry with support facilities, some visitor retail, and a couple small transient lodging businesses. The urban density area should have fully developed pedestrian and disabled compliant travel paths in the street right of way comprising a complete system.

Terrain is generally flat along the river then trends steeply upward at North Harbor Drive going into the City of Fort Bragg. Most pedestrian improvements are achievable in the flat terrain area and are considered “technically infeasible” on the steep slopes. Since arrival to this destination is primarily by vehicles due to steep road grades, pedestrian routes on the flat areas are a reasonable situation.

There will be challenges and coordination issues for improving the existing pedestrian infrastructure. Road system right of way width appears quite narrow and in a few locations the right of way edge appears to be on or within inches of building walls. Considerable congestion occurs during peak tourism days due to on street parked vehicles crowding the already narrow road lanes and pedestrians many times are also in the road lane due to parked vehicles obstructing right-of-way edges that should be used for walking purposes.

There are limited opportunities to intensify the development in this area due to the Coastal Plan limits. However since this is a known and popular visitor destination, businesses can be enhanced to increase income if planned pedestrian improvements are designed with a unifying theme and provide better walking circulation in the area. Particular attention should be paid to solving the existing parking congestion making the area more marketable to and desirable by visitors.



Mendocino County

Location:

Fort Bragg

CalTrans Map & Year:

2G1 1994

ADA Comprehensive Access Plan

Overview



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Fort Bragg

Category 1

Site Description:

An unincorporated urban density area adjacent to a City that is a visitor serving destination with restaurants, marine sports, a fishing village industry, limited retail, and small lodging accommodations.

Site Visit Pictures & Notes:



1: Crosswalk recommended at this general location to connect businesses on each street side into a continuous pedestrian route system. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.



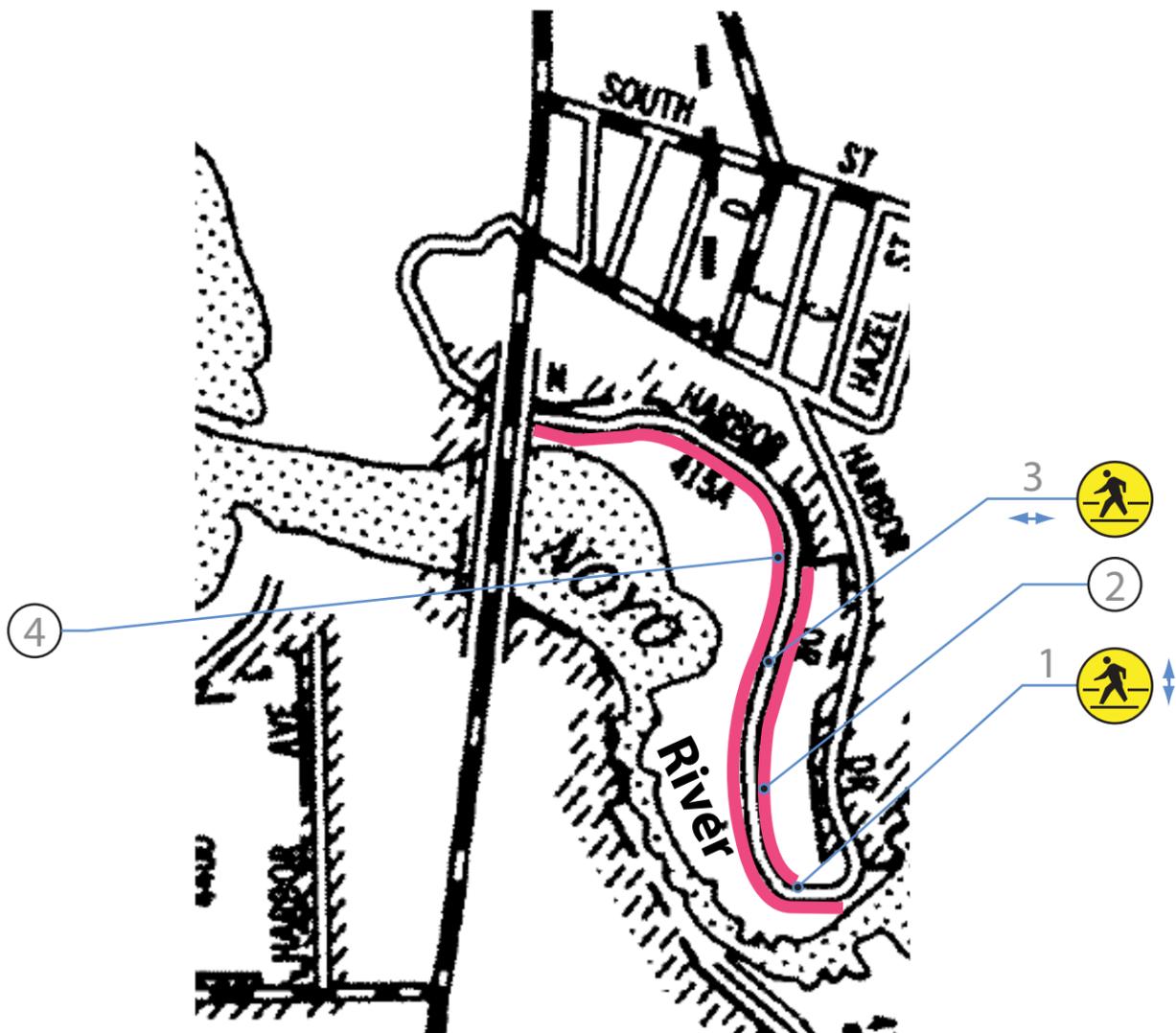
2: Sidewalk should be developed along this road portion. Current pedestrian walk is asphalt, dirt, and gravel with paving just wide enough for vehicle lanes. Portions are obstructed by parallel parked vehicles.



3: Crosswalk recommended at this general location to connect businesses on each street side into a continuous pedestrian route system. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.



4: Sidewalk should be developed along this road portion. Current pedestrian walk is asphalt, dirt, and gravel with paving just wide enough for vehicle lanes. Portions are obstructed by parallel parked vehicles.



Mendocino County

Location:
Fort Bragg

CalTrans Map & Year:
2G1 1994

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FUNCTIONAL CLASSIFICATION		URBAN		RURAL	
INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY	—————	INTERSTATE	—————	INTERSTATE	—————
OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————
MINOR ARTERIAL	—————	MINOR ARTERIAL	—————	MINOR ARTERIAL	—————
COLLECTOR	—————	MAJOR COLLECTOR	—————	MAJOR COLLECTOR	—————
LOCAL	—————	MINOR COLLECTOR	—————	MINOR COLLECTOR	—————
LOCAL	—————	LOCAL	—————	LOCAL	—————

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
- SW: Sidewalk Not Required				

Fort Bragg Photographs: Items 1 - 4



Mendocino County

Location:
Fort Bragg

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Access Plan

Photographs



Project #:
29075

Date:
06-30-10

Sheet:

Gualala

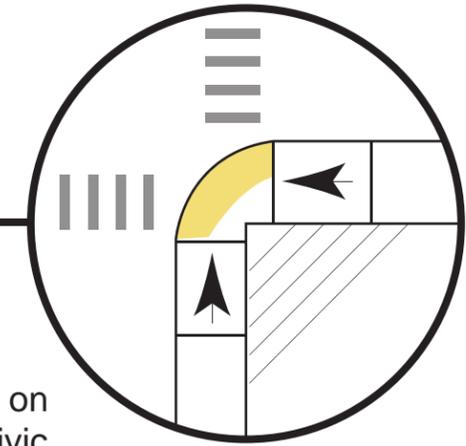
Overview

Category 3

This small coastal town is generally urban density for considering road right of way development along Highway 1 from Center Street on the south to Ocean Drive full intersection at the north. The town area is primarily tract density housing with commercial businesses, civic facilities, and visitor destinations. The medical and civic services sited east from Highway 1 are on steep slopes and not disabled pedestrian accessible. Residential areas north on Highway 1 are rugged terrain with home sites almost carved into the knolls and bluffs. Road signs on Highway 1 state “congested area next 1 mile” due to the resort and recreation facilities in the town area and nearby. It is notable that even though parking is prohibited along Highway 1, the very high parking demand and poorly defined off street parking lots results in many vehicles being parked along the road shoulder. Fully developed travel path is warranted along the commercial area on Highway 1.

Terrain in the urban density area is predominately gentle to moderate slopes with the Highway 1 shoreline side trending downward creating difficulty for pedestrian travel paths along the right of way or on a site. Terrain east of Highway 1 trends upward, again creating difficulty for developing pedestrian walk paths and adjacent property connections. Pedestrian travel paths are achievable but will have some difficulty to design along Highway 1. The existing pedestrian infrastructure is primarily the asphalt road shoulder having continuity interrupted by gravel or dirt portions with a few small sections that are older concrete sidewalks in aged condition and near end of service life.

Current land use zoning has limited opportunities to develop more commercial and industrial facilities in the urban density area. Higher density visitor serving commercial businesses may be allowed and connections to pedestrian travel paths serving newly redeveloped or altered facilities should be done with the projects as they develop.



Mendocino
County

Location:

Gualala

CalTrans Map & Year:

2H55 1994

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Overview



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Sheet:

Gualala

Category 3

Site Description:
A visitor destination coastal community situated along Highway 1 with a small commercial urban density area at the intersection of Ocean Drive and south along Highway 1 to Center Street.



Site Visit Pictures & Notes:

-  1: These locations are commonly used for crossing Highway 1 and are recommended as formal crosswalks. These recommended crosswalks should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
-  2: Pedestrian walks and intermittent sidewalks in this area have discontinuities and terrain slopes trending upward making design difficult. Currently the most available path is along the asphalt road shoulder. Due to intense traffic, it is recommended to have a design that separates pedestrians from traffic lanes such as parking lane between walkway and road lane. Design for pedestrian improvements need to consider how the pedestrian travel walkway will serve general travel along Highway 1, parked car locations, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage
-  3: Pedestrian walks and short run sidewalks in this area have discontinuities and terrain slopes trending upward making design difficult. The most achievable path in this area is the paved road shoulder. Traffic is intense in the area, it is recommended that pedestrian walkways be separate from road lanes. Design for pedestrian improvements need to consider how the pedestrian travel walkway will serve general travel along Highway 1, parked car locations, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage.
-  4: This location is commonly used for crossing Highway 1 and is recommended as a formal crosswalk. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
-  5: Pedestrian walks and intermittent sidewalks in this area have discontinuities and terrain slopes trend downward making design difficult. Currently the most available path is along the asphalt road shoulder. Due to intense traffic, it is recommended to have a design that separates pedestrians from traffic lanes such as parking lane between walkway and road lane. Design for pedestrian improvements need to consider how the pedestrian travel walkway will serve general travel along Highway 1, parked car locations, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage.



Mendocino County

Location:
Gualala

CalTrans Map & Year:
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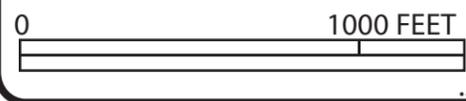


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FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		 - SW: Existing Non-Compliant Sidewalk  - SW: Provide Required Sidewalk  - SW: Existing Compliant Sidewalk  - SW: Sidewalk Not Required	 - CR: Not Provided (Required)  - CR: Existing Non-Compliant  - CR: Existing Compliant	 - Blended Transition  - Hazard  - Reduced Width	 - Crosswalk Recommended  - Crosswalk Existing  - Signage	 - Accessible Parking  - Transit Stop
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Gualala Photographs: Items 1 - 5



Mendocino County

Location:

Gualala

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Photographs



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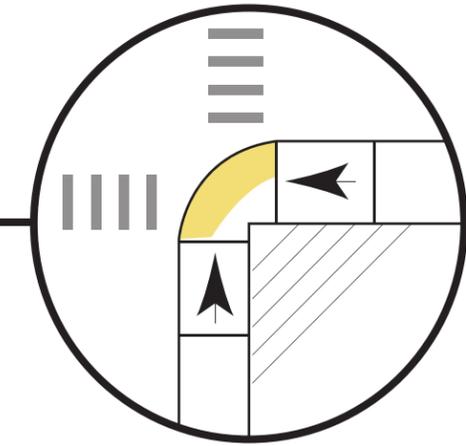
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Hopland

Overview

Category 3



This rural community has separate and distinct areas; Old Hopland east of US Highway 101 along State Highway 175, Hopland along US Highway 101 with commercial business development, and a southwest area across Feliz Creek that is predominately suburban to low density residential including an elementary school. The commercial area along US Highway 101 is urban density with existing developed pedestrian routes in the road right of way. Old Hopland is urban density residential with undeveloped pedestrian routes.

The only commercial facility in Old Hopland is the Sanel Valley Market at State Highway 175 near Howell Street intersection. Even though this can be considered an urban density village, there is no improved right of way with pedestrian sidewalks and the only walkways available are along the paved road shoulder. Disabled access is best served at this time by on site features for the Market. Consideration should be given to providing sidewalks or other pedestrian improvements when sufficient development occurs that could fund improving the existing area.

Terrain in the US Highway 101 urban density area is predominately gentle slopes with localized moderate steep portions. Currently the right of way is improved with sidewalks and other pedestrian travel features and should be repaired or reconstructed for disabled access compliance.

Current land use zoning will afford opportunities over time to develop more commercial facilities in the Old Hopland and Hopland urban density areas. Pedestrian travel path improvements in the road right of way serving new facilities should be done with the projects as they develop.



Mendocino
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Location:

Hopland

CalTrans Map & Year:

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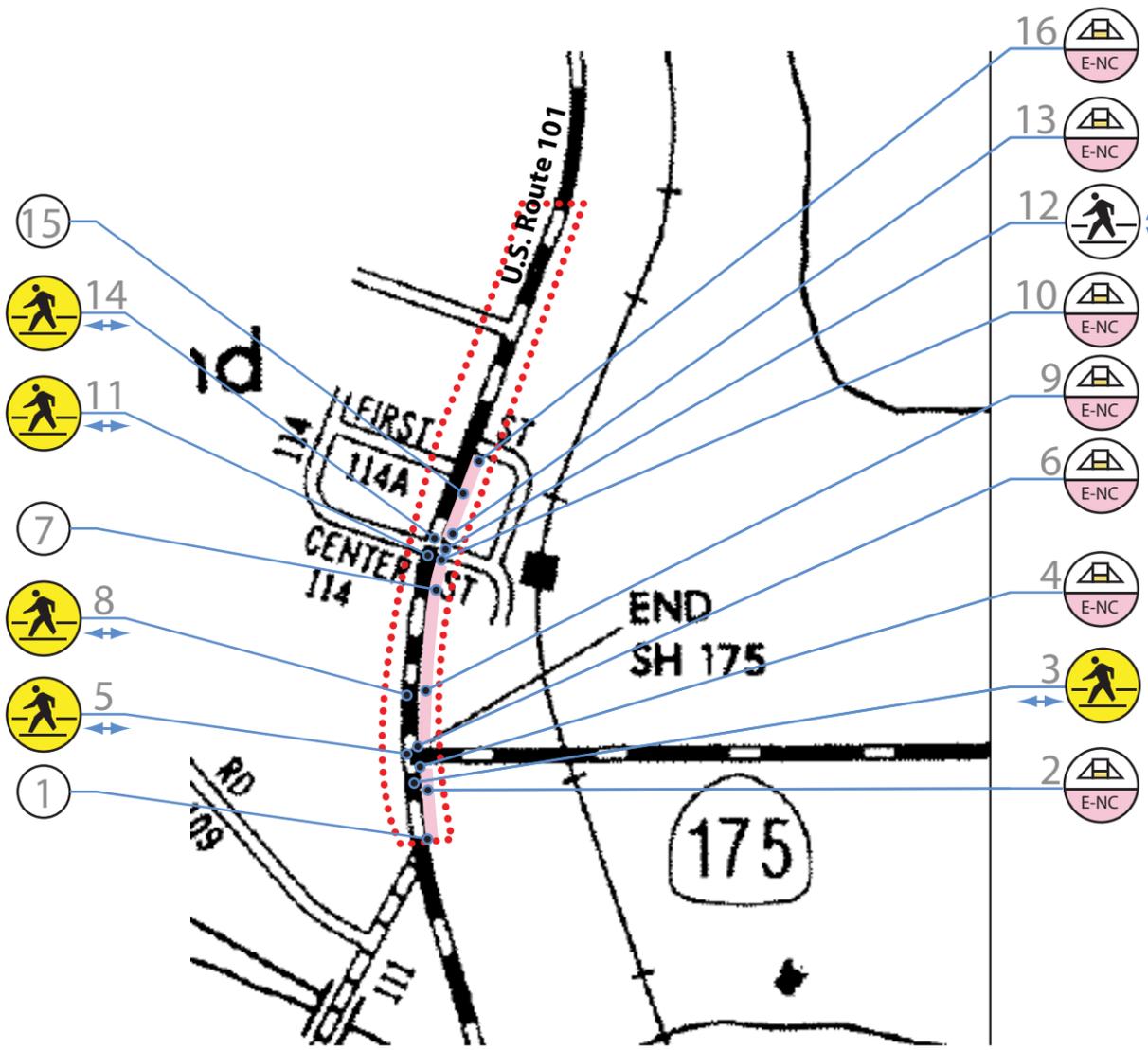
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Hopland

Category 3

Site Description:
A rural town having two separate developed town areas with an urban density commercial area located along US Highway 101.



Site Visit Pictures & Notes:

- 1: Existing sidewalk is a mix of rough asphalt with no defined travel direction boundaries and concrete driveway aprons. Reconstruction is necessary to correct rough surfaces, cross slopes, and afford travel path connections to the commercial business sites.
- 2: Drop down ramp at crosswalk; side ramps & bottom landing cross slopes exceed 2%, no detectable warnings, & ledge at landing to gutter. Reconstruction required.
- 3: Existing crosswalk; paving beyond gutter is very rough, & slope onto street through gutter exceeds 5%.
- 4: Curb ramp; it appears this ramp may have been intended to be a drop ramp design. Adjacent sidewalks and paving have cross slopes exceeding 2%, no level top landing for curb ramp (landing is on asphalt paving), no detectable warnings, and ledge at landing to gutter. Reconstruction required.
- 5: Existing crosswalk; paving beyond gutter is very rough, & slope onto street through gutter exceeds 5%. This is a high traffic intersection for US 101 and State 175, cross bar striping is recommended similar to the styles nearby crossing US 101 with the high visibility in lane pedestrian paddle signs.
- 6: Curb ramp; Adjacent sidewalks and paving are very rough, no detectable warnings, and ledge at landing to gutter. Reconstruction required.
- 7: Existing sidewalk is a mix of rough asphalt with no defined travel direction boundaries, dirt (undeveloped areas), relatively new concrete, concrete driveway aprons, and a historic board walkway at the historic hotel. Reconstruction is necessary to provide a continuous disabled compliant travel path, correct rough surfaces, excessive cross slopes, and afford travel path connections to the commercial business sites.
- 8: Existing crosswalk; paving beyond gutter is very rough, & slope onto street through gutter exceeds 5%.
- 9: Curb ramp; Adjacent sidewalks and paving have cross slopes exceeding 2%, no level top landing for curb ramp (obstructed by bollards and public payphone), no detectable warnings, and ledge at landing to gutter. Reconstruction required.
- 10: Curb ramp; no detectable warnings, ledge at landing to gutter, and gutter slope exceeds 5%. Reconstruction required.
- 11: Existing crosswalk; Misaligned to diagonal curb ramp, slope near gutter exceeds 5%, & rough paving.
- 12: Crosswalk; recommend striping to define crosswalk area and landing transition areas at curb ramp bottoms. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is rough and should be overlaid for pedestrian and disabled travel path.
- 13: Curb ramp; Adjacent sidewalks and paving have cross slopes exceeding 2%, no level top landing for curb ramp, no detectable warnings, and ledge at landing to gutter. Reconstruction required.
- 14: Existing crosswalk; Misaligned to diagonal curb ramp, slope near gutter exceeds 5%, & rough paving.
- 15: Existing sidewalk is a mix of older concrete with a patch of brick near First St. intersection. Reconstruction is necessary to correct cross slopes exceeding 2%, rough areas, cracks, and ledges.
- 16: Curb ramp; top landing not within 2% run and cross slope, ramp run slope exceeds 8.33%, detectable warning not full width, irregular surface at bottom; reconstruction required.



Mendocino County

Location:
Hopland

CalTrans Map & Year:
3H34 1994

ADA Comprehensive Access Plan



Project #:
29075

Date:
05-28-10

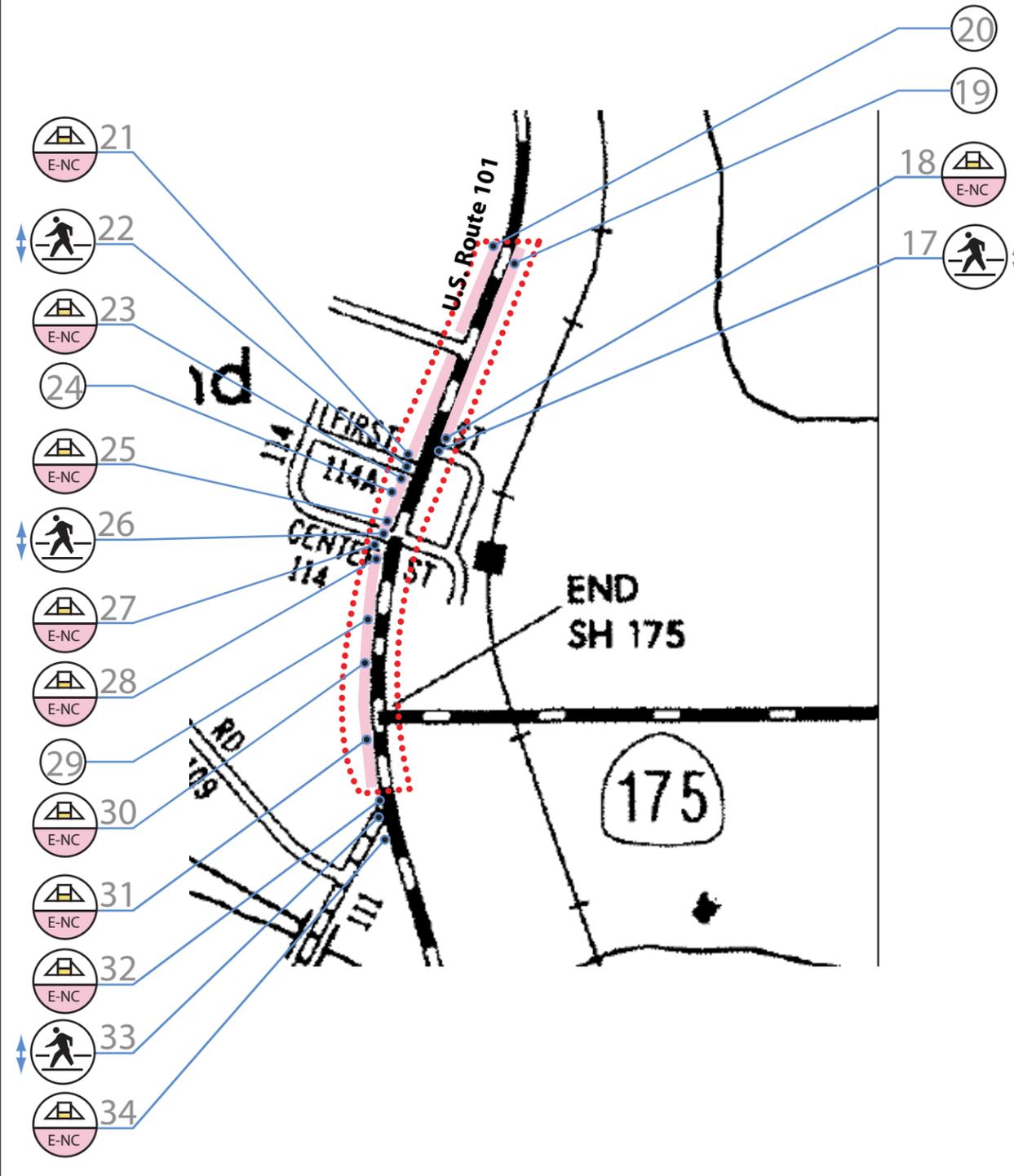
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<p>FUNCTIONAL CLASSIFICATION</p> <p>URBAN</p> <p>INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY</p> <p>OTHER PRINCIPAL ARTERIAL</p> <p>MINOR ARTERIAL</p> <p>COLLECTOR</p> <p>LOCAL</p>	<p>RURAL</p> <p>INTERSTATE</p> <p>OTHER PRINCIPAL ARTERIAL</p> <p>MINOR ARTERIAL</p> <p>MAJOR COLLECTOR</p> <p>MINOR COLLECTOR</p> <p>LOCAL</p>	<p> - SW: Existing Non-Compliant Sidewalk</p> <p> - SW: Provide Required Sidewalk</p> <p> - SW: Existing Compliant Sidewalk</p> <p> - SW: Sidewalk Not Required</p>	<p> - CR: Not Provided (Required)</p> <p> - CR: Existing Non-Compliant</p> <p> - CR: Existing Compliant</p>	<p> - Blended Transition</p> <p> - Hazard</p> <p> - Reduced Width</p>	<p> - Crosswalk Recommended</p> <p> - Crosswalk Existing</p> <p> - Signage</p>	<p> - Accessible Parking</p> <p> - Transit Stop</p>
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Hopland

Category 3

Site Description:
A rural town having two separate developed town areas with an urban density commercial area located along US Highway 101.



Site Visit Pictures & Notes:

-  17: Crosswalk; recommend striping to define crosswalk area. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is rough and should be overlaid for pedestrian and disabled travel path.
-  18: Curb ramp; no top landing provided (undeveloped sidewalk beyond), ramp cross slope exceeds 2%, ledge from ramp bottom to gutter, and gutter slope (run) exceeds 5%; reconstruction required.
-  19: Sidewalk; existing composition is dirt (some undeveloped portions), concrete in relatively good condition, older concrete portions in poor condition, numerous driveway aprons, and patches of old asphalt, Reconstruction is necessary to provide a continuous disabled compliant travel path, correct cross slopes exceeding 2%, rough areas, cracks, and ledges.
-  20: Sidewalk; this portion is primarily residential and provides parking for the commercial area, it is comprised mostly of dirt (undeveloped portions), numerous rough driveway aprons, and a small area near first street with older concrete in fair condition. Reconstruction and new sidewalk installation is required to provide a continuous pedestrian and disabled compliant travel path.
-  21: Curb ramp; top landing not within 2% run and cross slope, ledge at ramp bottom to gutter pan, non-typical grooves at ramp sides, gutter run slope exceeds 5%, diagonal layout doesn't afford crossing alignment with allowed crossing direction; reconstruction required.
-  22: Crosswalk; recommend striping to define crosswalk area. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is rough and should be overlaid for pedestrian and disabled travel path.
-  23: In line ramp at corner; handrails stop before ramp slope ends, handrail extensions are not 12" graspable due to return bends, bottom landing is a crosswalk with irregular and rough paving, bottom landing is not level (max 2% run and cross slope). Reconstruction required.
-  24: Sidewalk; existing is concrete from corner to corner with a newer inline ramp at first street then older concrete in fair condition to Center St. Reconstruction required to correct excessive cross slopes and ledges.
-  25: Curb ramp; no detectable warnings, ledge at ramp bottom to gutter pan, gutter run slope exceeds 5%, diagonal layout doesn't afford crossing alignment with allowed crossing direction; reconstruction required.
-  26: Crosswalk; recommend striping to define crosswalk area and landing transition areas at curb ramp bottoms. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is rough and should be overlaid for pedestrian and disabled travel path.
-  27: Curb ramp; no detectable warnings, top landing not 48" deep, ledge at ramp bottom to gutter pan, gutter run slope exceeds 5%, diagonal layout doesn't afford crossing alignment with allowed crossing direction; reconstruction required.
-  28: Curb ramp; no detectable warnings, top landing not 48" deep, ledge at ramp bottom to gutter pan, gutter run slope exceeds 5%; reconstruction required.
-  29: Sidewalk; existing construction is comprised of mostly concrete with a few portions dirt (undeveloped), the area near State 175 intersection is in relatively good condition and disabled compliant except at curb ramps and discontinuity at Mountain House Road intersection.
-  30: Curb ramp; currently comprised of sloped dirt in an undeveloped sidewalk area, need curb ramp constructed.
-  31: Drop down ramp at crosswalk; side ramps & bottom landing cross slopes exceed 2%, no detectable warnings, & ledge at landing to gutter. Reconstruction required.
-  32 & 34: Pedestrian path is discontinuous for disabled compliant travel path, provide sidewalk extension and curb ramp. Pedestrian path is discontinuous for disabled compliant travel path, provide curb ramp and sidewalk extension affording travel path connection to the business site.
-  30: Crosswalk; recommend striped crosswalk with cross bars and high visibility pedestrian paddle signs.

<p>FUNCTIONAL CLASSIFICATION</p> <table style="width: 100%; border: none;"> <tr> <th style="text-align: left;">URBAN</th> <th style="text-align: left;">RURAL</th> </tr> <tr> <td>INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY</td> <td>INTERSTATE</td> </tr> <tr> <td>OTHER PRINCIPAL ARTERIAL</td> <td>OTHER PRINCIPAL ARTERIAL</td> </tr> <tr> <td>MINOR ARTERIAL</td> <td>MINOR ARTERIAL</td> </tr> <tr> <td>COLLECTOR</td> <td>MAJOR COLLECTOR</td> </tr> <tr> <td>LOCAL</td> <td>MINOR COLLECTOR</td> </tr> <tr> <td>LOCAL</td> <td>LOCAL</td> </tr> </table>	URBAN	RURAL	INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY	INTERSTATE	OTHER PRINCIPAL ARTERIAL	OTHER PRINCIPAL ARTERIAL	MINOR ARTERIAL	MINOR ARTERIAL	COLLECTOR	MAJOR COLLECTOR	LOCAL	MINOR COLLECTOR	LOCAL	LOCAL	<p> - SW: Existing Non-Compliant Sidewalk</p> <p> - SW: Provide Required Sidewalk</p> <p> - SW: Existing Compliant Sidewalk</p> <p> - SW: Sidewalk Not Required</p>	<p> - CR: Not Provided (Required)</p> <p> - CR: Existing Non-Compliant</p> <p> - CR: Existing Compliant</p>	<p> - Blended Transition</p> <p> - Hazard</p> <p> - Reduced Width</p>	<p> - Crosswalk Recommended</p> <p> - Crosswalk Existing</p> <p> - Signage</p>	<p> - Accessible Parking</p> <p> - Transit Stop</p>
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INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY	INTERSTATE																		
OTHER PRINCIPAL ARTERIAL	OTHER PRINCIPAL ARTERIAL																		
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LOCAL	MINOR COLLECTOR																		
LOCAL	LOCAL																		



Mendocino County

Location:
Hopland

CalTrans Map & Year:
3H34 1994

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Project #:
29075

Date:
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Hopland Photographs: Items 1 - 26



Mendocino County

Location:
Hopland

ADA
Comprehensive
Access Plan

Photographs

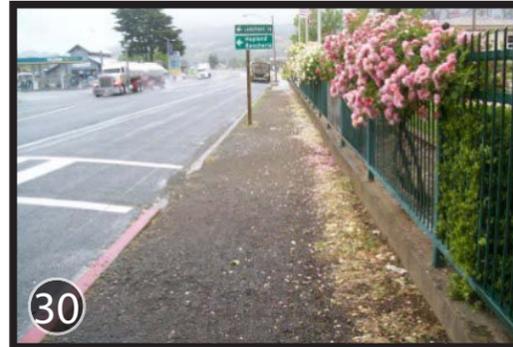


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Hopland Photographs: Items 27 - 34



Mendocino
County

Location:

Hopland

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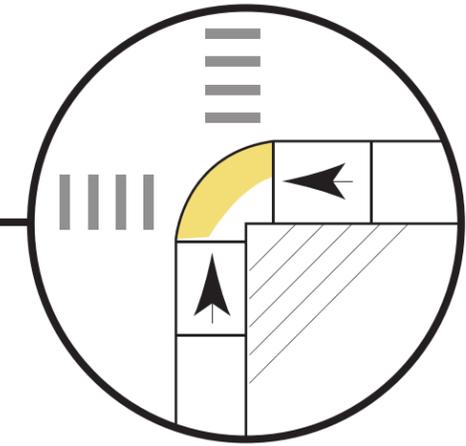
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Laytonville

Overview

Category 3



This rural town is a distinct and compact self contained community for housing and commercial businesses. The town area is primarily tract density housing with some commercial and civic facilities. Laytonville is generally urban density for considering road right of way development along US 101 from Ramsey Road on the south to about one block north of Branscomb Road intersection. The other commercial and visitor serving businesses north and south of the urban density area are separated by pedestrian travel distances exceeding 500 feet.

Terrain in the urban density area is gentle to moderate slopes with the area west of US 101 trending downward creating difficulty for pedestrian travel path along the right of way or on a site. Pedestrian travel paths are achievable along US 101. Recent projects have resulted in relatively new concrete curb gutter and sidewalks for a significant length along US 101. There are some gaps remaining in the travel paths that need connections to form a completed pedestrian system.

Current land use zoning will afford opportunities over time to develop more commercial and industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:

Laytonville

CalTrans Map & Year:

3F41 1994

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Overview



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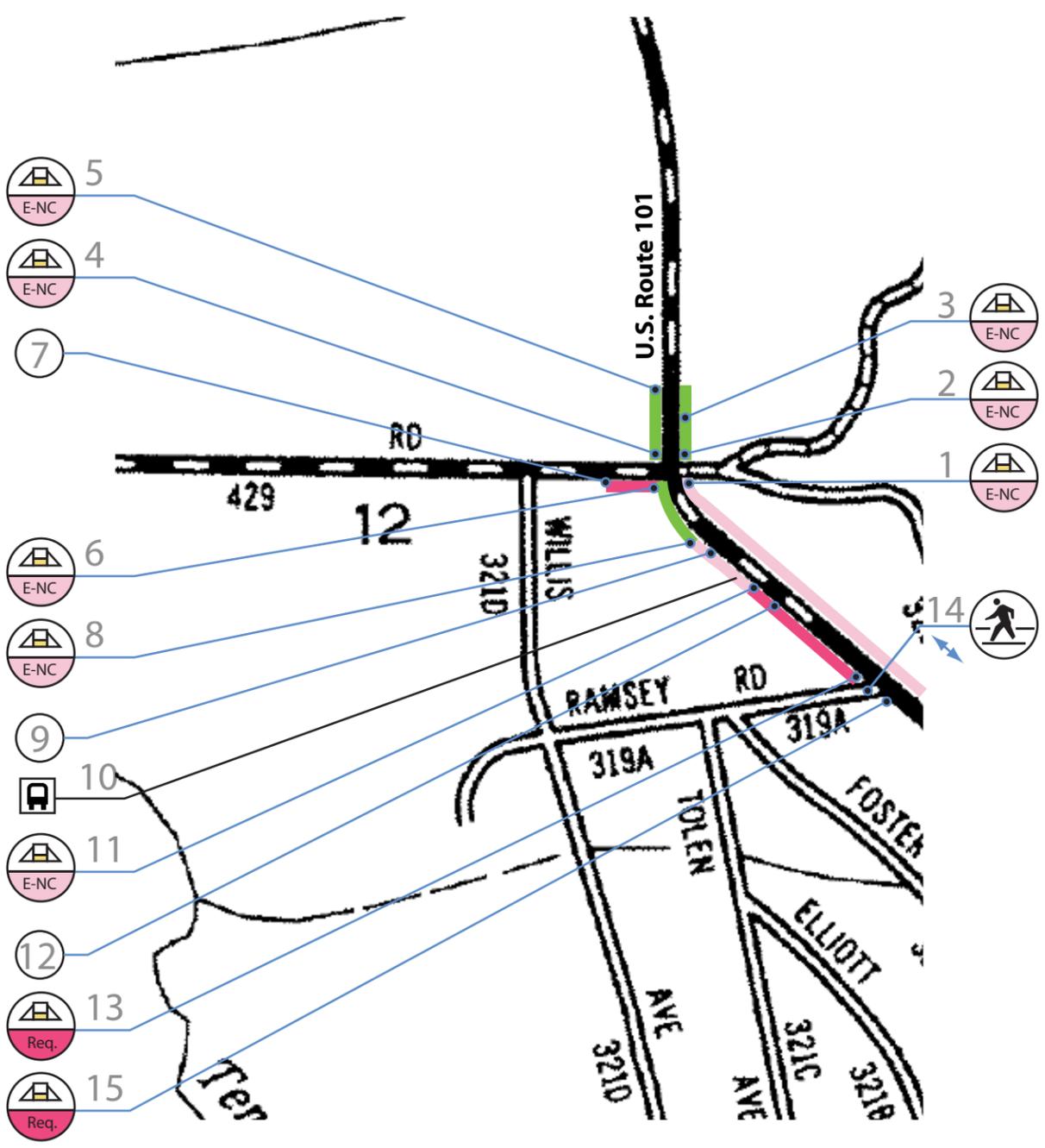
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Laytonville

Category 3

Site Description:
A rural town situated along US 101 with a small commercial urban density area at the intersection of Branscomb Road and south along US 101 to Ramsey Road.



Site Visit Pictures & Notes:

- 1: Curb ramp has silt and soil debris, slippery footing conditions; drainage needs corrected. Layout at this corner is more a blended transition than diagonal curb ramp as there is no sidewalk around the corner. Best practice is a perpendicular transition at curb ramp bottom, this ramp has a curved bottom transition from ramp surface to gutter that can cause uneven wheelchair wheel contact.
- 2: Layout at this corner is more a blended transition than diagonal curb ramp as there is no sidewalk around the corner. Best practice is a perpendicular transition at curb ramp bottom, this ramp has a curved bottom transition from ramp surface to gutter that can cause uneven wheelchair wheel contact.
- 3: Curb ramp has silt and soil debris, slippery footing conditions; drainage needs corrected. Ledge at curb to crosswalk brick paving.
- 4: Curb ramp has silt and soil debris, slippery footing conditions; drainage needs corrected.
- 5: Layout at this corner is more a blended transition than diagonal curb ramp as there is no sidewalk around the corner. Best practice is a perpendicular transition at curb ramp bottom, this ramp has a curved bottom transition from ramp surface to gutter that can cause uneven wheelchair wheel contact.
- 6: Curb ramp has silt and soil debris, slippery footing conditions; drainage needs corrected. Ledge at curb to crosswalk brick paving.
- 7: Sidewalk or pedestrian path required on this street side to connect the health center with the urban density travel paths. Current path is along the asphalt road shoulder without a designated walkway. Pedestrian path can be along the asphalt road shoulder if there is sufficient width and proper slopes. Durable striping recommended to delineate vehicle driveways and walk areas.
- 8: Curb ramp has silt and soil debris, slippery footing conditions; drainage needs corrected. Ledge at curb to crosswalk brick paving.
- 9: Sidewalk is not continuous, existing concrete is not in good condition, asphalt portions have excessive cross slopes, transitions from concrete to asphalt are excessive steep.
- 10: Bus stop; insufficient sidewalk width for bus to deploy a wheelchair ramp.
- 11: Curb ramp; no detectable warnings, lip at ramp bottom to gutter, excessive gutter slope, excessive bottom landing slope.
- 12: Sidewalk or pedestrian path required on this street side. Current path is along the asphalt road shoulder without a designated walkway. Pedestrian path can be along the asphalt road shoulder if there is sufficient width and proper slopes. Since the shoulder is higher than adjacent properties, an alternate is to develop the pedestrian path along right of way boundary at lower level. Durable striping recommended to delineate vehicle driveways and walk areas.
- 13: Curb ramp or blended transition is missing at this corner and is required to provide a continuous pedestrian and disabled access travel path.
- 14: Crossing; striping recommended due to high side road traffic. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 15: Curb ramp or blended transition is missing at this corner and is required to provide a continuous pedestrian and disabled access travel path.



Mendocino County

Location:
Laytonville

CalTrans Map & Year:
3F41 1994

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Access Plan

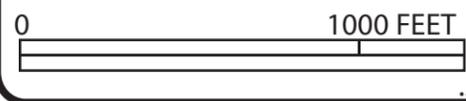


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Date:
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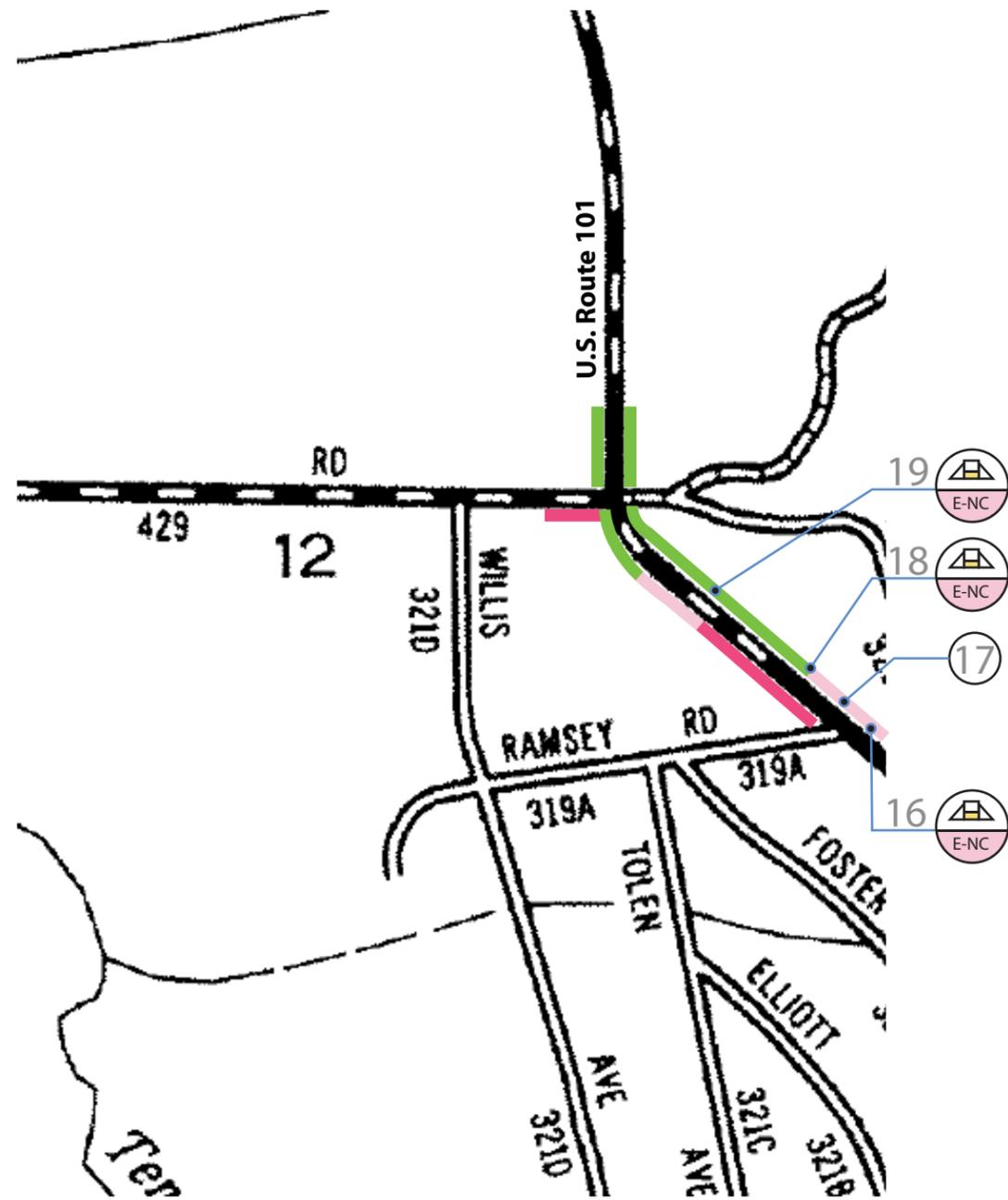
FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Laytonville

Category 3

Site Description:
A rural town situated along US 101 with a small commercial urban density area at the intersection of Branscomb Road and south along US 101 to Ramsey Road.



Site Visit Pictures & Notes:



16: Curb ramp or blended transition is missing at this "tee" intersection and is required to provide a continuous pedestrian and disabled access travel path.



17: Sidewalk is not continuous, existing concrete is not in good condition, asphalt portions have excessive cross slopes.



18: Layout at this corner is more a blended transition than diagonal curb ramp as there is no sidewalk around the corner. Best practice is a perpendicular transition at curb ramp bottom, this ramp has a curved bottom transition from ramp surface to gutter that can cause uneven wheelchair wheel contact.



19: Curb ramp has silt and soil debris, slippery footing conditions; drainage needs corrected. No detectable warnings visible or useable.



Mendocino County

Location:

Laytonville

CalTrans Map & Year:

3F41 1994

ADA Comprehensive Access Plan



Ref. North

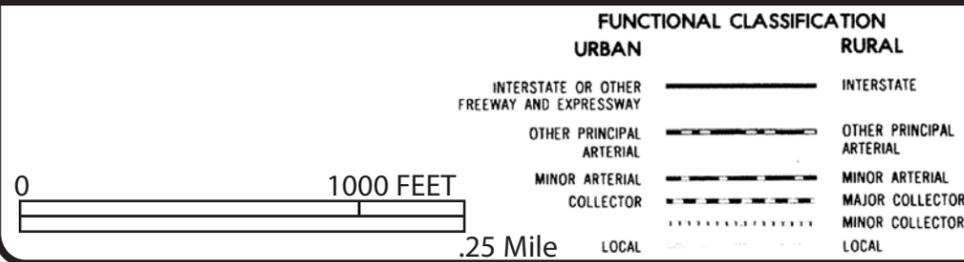


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Project #: 29075

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- SW: Existing Non-Compliant Sidewalk
- SW: Provide Required Sidewalk
- SW: Existing Compliant Sidewalk
- SW: Sidewalk Not Required

- CR: Not Provided (Required)
- CR: Existing Non-Compliant
- CR: Existing Compliant

- Blended Transition
- Hazard
- Reduced Width

- Crosswalk Recommended
- Crosswalk Existing
- Signage

- Accessible Parking
- Transit Stop

Laytonville Photographs: Items 1 - 19



Mendocino County

Location:

Laytonville

ADA Comprehensive Access Plan

Photographs



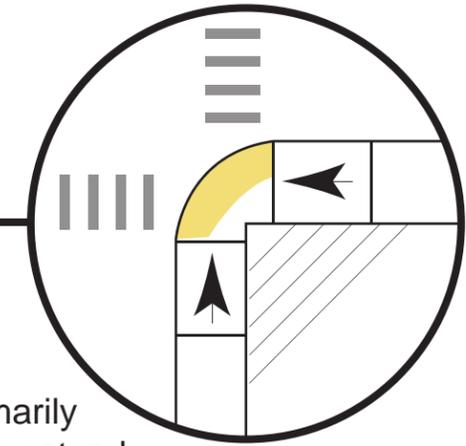
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06-30-10

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Town of Mendocino



Overview

Category 3

This small rural town is urban density throughout the area west of Highway 1. Areas east of Highway 1 are rural to urban density, primarily residential. This community is a nationally known tourist destination and is a unique setting for architectural character in a spectacular natural environment. There are a number of visitor serving businesses and small lodging facilities that attract tourists throughout the year. Traffic can be intense and highly congested during peak travel season. Of special note is the grid block original town area being on the national register for historic places.

Terrain in the historic village area is gentle to moderate slopes with localized steep slopes. Pedestrian infrastructure has been developed over time in an uncoordinated manner and currently is comprised of many varieties such as dirt, gravel, concrete, unit paving stones, bricks, wood planks, and stairs. Travel routes are not continuous for affording disabled pedestrian access and there are localized steep street grades that are considered technically infeasible for improving pedestrian travel paths in the street right-of-way. Most other locations with gentle to moderate slopes are achievable for improving the pedestrian routes given careful design. Since the historic preservation for this community includes the public right-of-way, there will be special challenges and issues for designing pedestrian infrastructure systems.

The major concerns for pedestrian improvements are; the interference caused by vehicle parking due to right-of-way widths available, parking space demand that far exceeds capacity, and intense pedestrian use by tourists during peak travel season. Solutions to these concerns will require a coordinated effort with the Historic Board, Planning, and Department of Transportation.

Current land use zoning affords limited and highly restricted opportunities over time to develop more commercial or residential facilities in the urban density historic area. Some development may occur if in compliance with community planning goals, architectural design standards, and does not impact the community character or historic significance. Most pedestrian infrastructure improvements are anticipated to be by capital improvement programs or similar, and less reliant on new or intensified developments providing off-site work.



Mendocino County

Location:

Mendocino

CalTrans Map & Year:

2G1 1994

2G43 1994

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Overview



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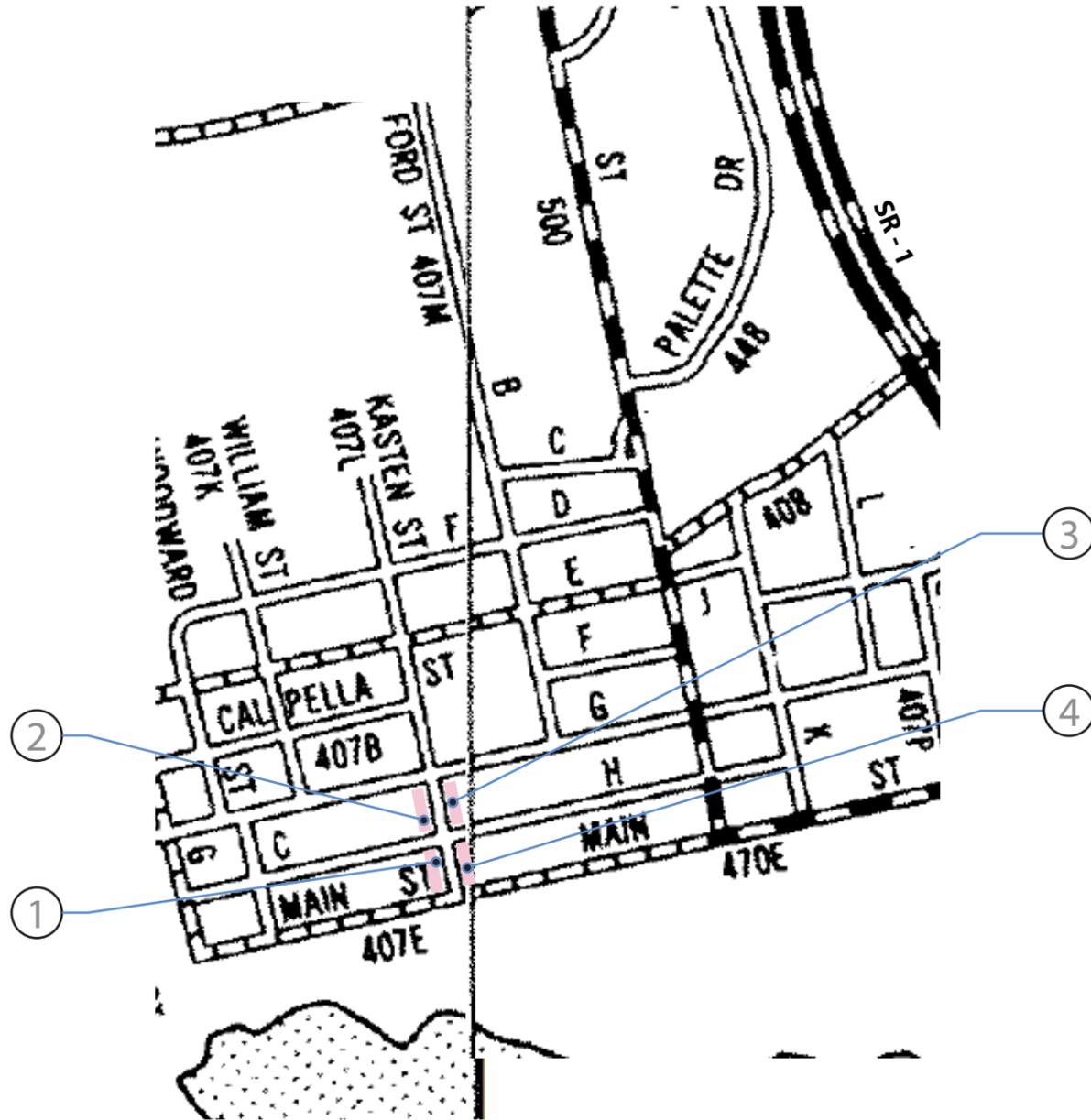
05-28-10

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Town of Mendocino

Category 3: Kasten St.

Site Description:
A nationally known visitor destination rural town situated west of Highway 1 with a well developed commercial urban density core comprised of historic buildings with unique architecture.



Site Visit Pictures & Notes:

- 1: Between Main and Albion, there are moderate to steep sidewalk slopes; persons with mobility impairments should be directed to another route. Sidewalk surface has some irregular surfaces at cracks that will require maintenance soon and portions with excessive cross slopes that should be corrected to direct water away from buildings.
- 2: Between Albion and Ukiah, the available walking route is obstructed by parallel parking, has excessive cross slope, has abrupt vertical changes (steps & ledges), and is obstructed along right-of-way edge by landscaping. It appears the street grade is shallow enough to afford mobility impaired travel path.
- 3: Between Ukiah and Albion, the available walking route is obstructed by parallel parking, has excessive cross slope, is not firm stable and slip resistant, and is obstructed along right-of-way edge by landscaping. It appears the street grade is shallow enough to afford mobility impaired travel path.
- 4: Between Albion and Main, there are moderate to steep sidewalk slopes; persons with mobility impairments should be directed to another route. Sidewalk surface have excessive cross slopes that should be corrected to direct water away from buildings.



Mendocino County

Location:
Mendocino
Kasten St.

CalTrans Map & Year:
2G1 1994
2G43 1994

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FUNCTIONAL CLASSIFICATION		URBAN		RURAL	
INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY	—————	INTERSTATE	—————	INTERSTATE	—————
OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————
MINOR ARTERIAL	—————	MINOR ARTERIAL	—————	MINOR ARTERIAL	—————
COLLECTOR	—————	MAJOR COLLECTOR	—————	MAJOR COLLECTOR	—————
LOCAL	—————	MINOR COLLECTOR	—————	MINOR COLLECTOR	—————
LOCAL	—————	LOCAL	—————	LOCAL	—————

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
- SW: Sidewalk Not Required				

Town of Mendocino Photographs: Items 1 - 4 Kasten Street



Mendocino County

Location:
Town of Mendocino
Kasten St.

ADA
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Access Plan

Photographs



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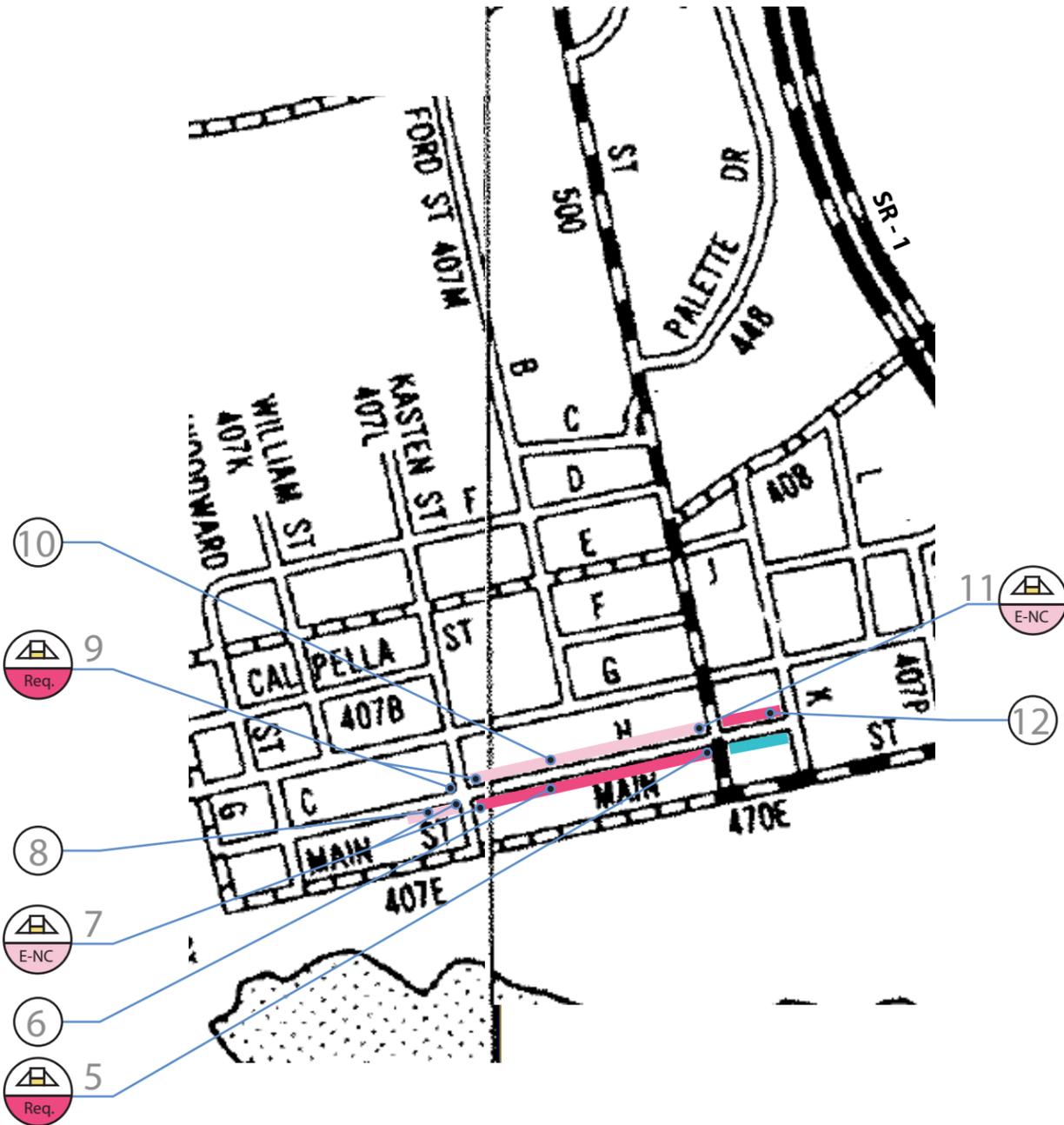
Town of Mendocino

Category 3: Albion St.

Site Description:

A nationally known visitor destination rural town situated west of Highway 1 with a well developed commercial urban density core comprised of historic buildings with unique architecture.

Site Visit Pictures & Notes:



5: Curb ramp or blended transition is required at this corner for continuous pedestrian travel path.



6: Sidewalk is required along this block to afford a continuous pedestrian travel path. The available walking route is obstructed by parallel parking, has excessive cross slope, is not firm stable and slip resistant, has difficult transitions for walkway connections at adjacent properties, and right-of-way may not be wide enough to afford vehicle parking.



7: Curb ramps or blended transitions are not code compliant at these corners for continuous pedestrian travel path. Repair or reconstruction required.



8: Sidewalk is required along this block portion to afford a continuous pedestrian travel path connecting the corner with businesses. The available walking route is obstructed by parallel parking, has excessive cross slope, is not firm stable and slip resistant, and right-of-way may not be wide enough to afford vehicle parking.



9: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.



10: Existing sidewalk; is in aged condition requiring corrective work, not continuous, not wide enough for code compliance, portions have excessive cross slope, is obstructed by parallel parking, is not firm stable and slip resistant, has a wood surface portion in poor condition, and right-of-way may not be wide enough to afford vehicle parking.



11: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.



12: Sidewalk is required along this block portion to afford a continuous pedestrian travel path. The available walking route is obstructed by parallel parking, has excessive cross slope, is obstructed by landscaping, and right-of-way may not be wide enough to afford vehicle parking.



Mendocino County

Location:
Mendocino
Albion St.

CalTrans Map & Year:
2G1 1994
2G43 1994

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Access Plan



Ref. North

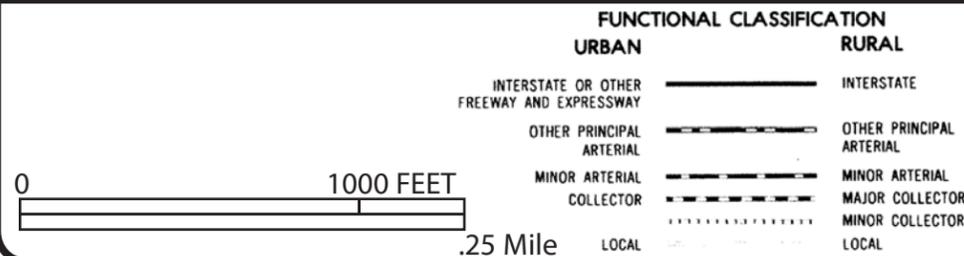


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- SW: Existing Non-Compliant Sidewalk
- SW: Provide Required Sidewalk
- SW: Existing Compliant Sidewalk
- SW: Sidewalk Not Required

- CR: Not Provided (Required)
- CR: Existing Non-Compliant
- CR: Existing Compliant

- Blended Transition
- Hazard
- Reduced Width

- Crosswalk Recommended
- Crosswalk Existing
- Signage

- Accessible Parking
- Transit Stop

Town of Mendocino Photographs: Items 5 - 12 Albion Street



Mendocino County

Location:
Town of Mendocino
Albion St.

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Town of Mendocino

Category 3: Howard St.

Site Description:

A nationally known visitor destination rural town situated west of Highway 1 with a well developed commercial urban density core comprised of historic buildings with unique architecture.

Site Visit Pictures & Notes:



13: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking, and is mostly undeveloped at this time. Block portion between Albion to Main on east side may be too steep for disabled travel path.



14: Curb ramp or blended transition is required at this corner for continuous pedestrian travel path.



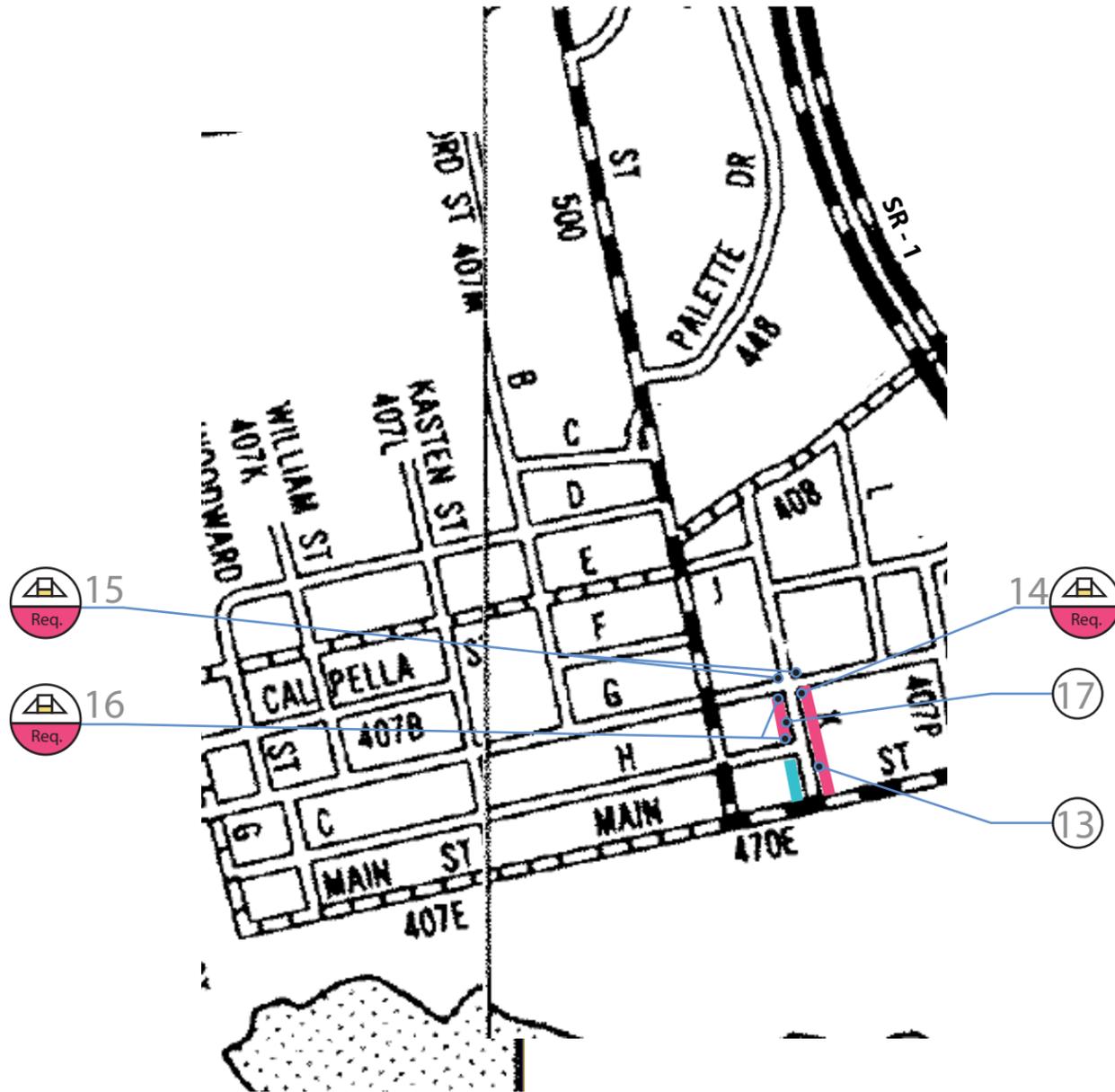
15: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.



16: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.



17: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking, and is mostly undeveloped at this time.



Mendocino County

Location:
Mendocino
Howard St.

CalTrans Map & Year:
2G1 1994
2G43 1994

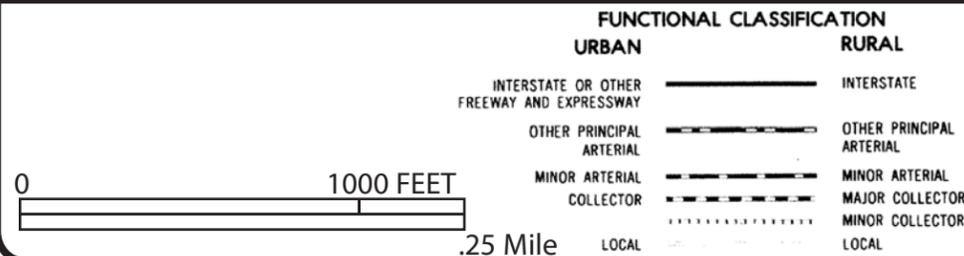
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Comprehensive
Access Plan



Project #:
29075

Date:
05-28-10

Sheet:



- SW: Existing Non-Compliant Sidewalk
- SW: Provide Required Sidewalk
- SW: Existing Compliant Sidewalk
- SW: Sidewalk Not Required

- CR: Not Provided (Required)
- CR: Existing Non-Compliant
- CR: Existing Compliant

- Blended Transition
- Hazard
- Reduced Width

- Crosswalk Recommended
- Crosswalk Existing
- Signage

- Accessible Parking
- Transit Stop

Town of Mendocino Photographs: Items 13 - 17 Howard Street



Mendocino
County

Location:
Town of
Mendocino
Howard St

ADA
Comprehensive
Access Plan

Photographs



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Town of Mendocino

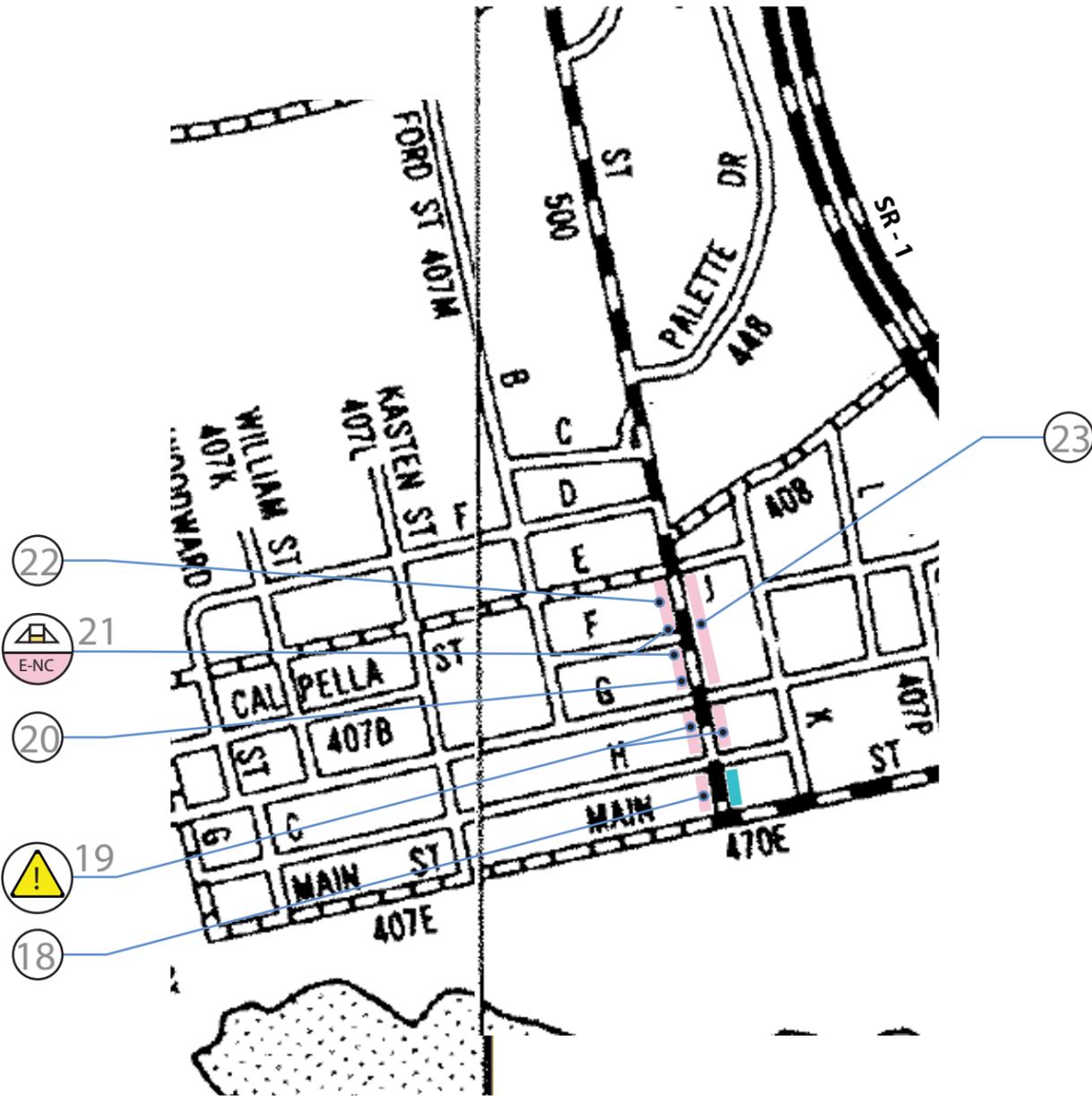
Category 3: Lansing St.

Site Description:

A nationally known visitor destination rural town situated west of Highway 1 with a well developed commercial urban density core comprised of historic buildings with unique architecture.

Site Visit Pictures & Notes:

- 18: Existing sidewalk is compliant for the southerly part along the building wall. Signs should be posted for directions to accessible sidewalks or other route at the steeper northerly part. Disabled parking should be provided at the street "nose in" parking spaces.
- 19: These sidewalks are too steep for mobility impaired persons, alternate routes need to be considered and proper signage for route directions posted. Stairways need to be rebuilt for code compliance with level footing on treads and handrails.
- 20: Sidewalk exists along this block with the available route obstructed by a wood stair and plank walkway at northwest corner of Lansing and Ukiah. Portions are narrower than 4 ft and obstructed by landscaping that should be trimmed.
- 21: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.
- 22: Sidewalk exists along this block and portions are in poor condition for cross slope and ledges and cracking. At northwest corner of Lansing and Calpella, the sidewalk exceeds 5% and is steeper than the road grade, corrective work required.
- 23: Sidewalk exists along this block with northerly part in aged but serviceable condition, and the balance requiring corrective work for cracks, ledges, and excessive cross slope.



Mendocino County

Location:
Mendocino
Lansing St.

CalTrans Map & Year:
2G1 1994
2G43 1994

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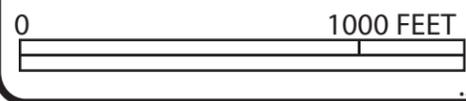
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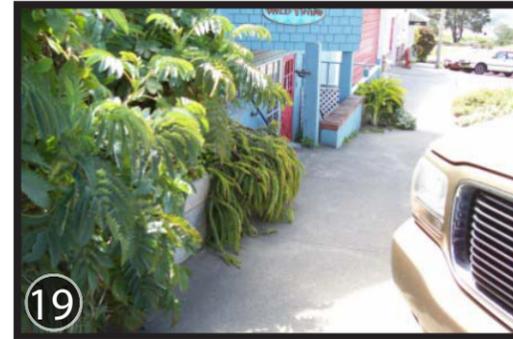
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FUNCTIONAL CLASSIFICATION		URBAN		RURAL	
INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY	—————	INTERSTATE	—————	INTERSTATE	—————
OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————
MINOR ARTERIAL	—————	MINOR ARTERIAL	—————	MINOR ARTERIAL	—————
COLLECTOR	—————	MAJOR COLLECTOR	—————	MAJOR COLLECTOR	—————
LOCAL	—————	MINOR COLLECTOR	—————	MINOR COLLECTOR	—————
LOCAL	—————	LOCAL	—————	LOCAL	—————

- SW: Existing Non-Compliant Sidewalk	- SW: Provide Required Sidewalk	- SW: Existing Compliant Sidewalk	- SW: Sidewalk Not Required
- CR: Not Provided (Required)	- CR: Existing Non-Compliant	- CR: Existing Compliant	
- Blended Transition	- Hazard	- Reduced Width	
- Crosswalk Recommended	- Crosswalk Exisiting	- Signage	
- Accessible Parking	- Transit Stop		



Town of Mendocino Photographs: Items 18 - 23 Lansing Street



Mendocino County

Location:
Town of Mendocino
Lansing St.

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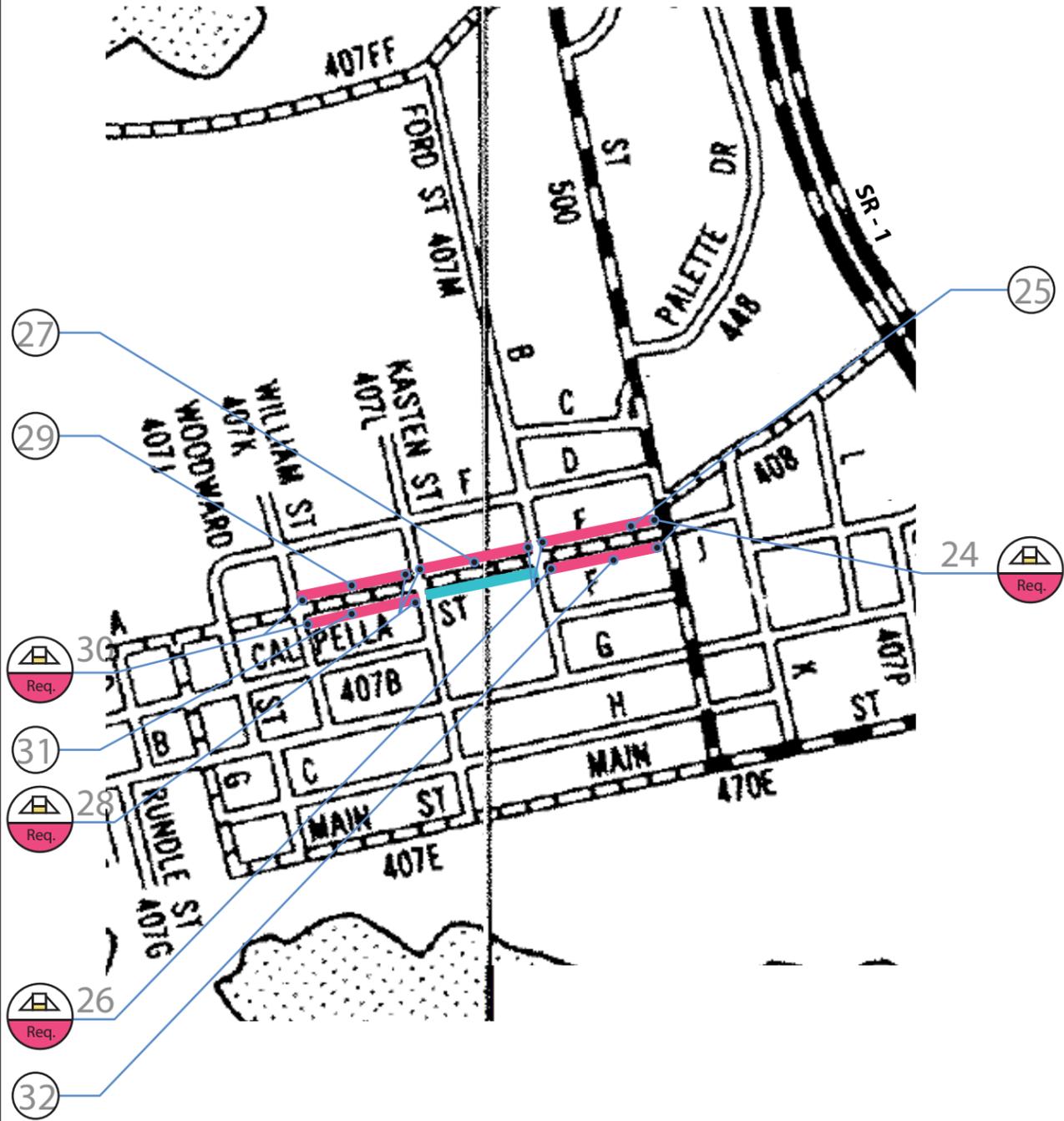
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Town of Mendocino

Category 3: Little Lake St.

Site Description:

An nationally known visitor destination rural town situated west of Highway 1 with a well developed commercial urban density core comprised of historic buildings and unique architecture.



Site Visit Pictures & Notes:

- 24: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.
- 25: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking, and is mostly undeveloped at this time.
- 26: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.
- 27: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking, is mostly undeveloped, and existing concrete walks are in poor condition.
- 28: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.
- 29: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking, is partly undeveloped, and existing concrete walks are in poor condition.
- 30: Curb ramps or blended transitions are required at these corners for continuous pedestrian travel path.
- 31: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking and is mostly undeveloped.
- 32: Sidewalk is required along this block to provide a continuous pedestrian travel path. The available walking route is obstructed by parking, is partly undeveloped, and existing walks are either not compliant or in poor condition.



Mendocino County

Location:
Mendocino
Little Lake St.

CalTrans Map & Year:
2G1 1994
2G43 1994

ADA
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Access Plan



Project #:
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Date:
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FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Town of Mendocino Photographs: Items 24 - 32 Little Lake Street



Mendocino County

Location:
Town of Mendocino
Little Lake St.

ADA
Comprehensive
Access Plan

Photographs



Project #:
29075

Date:
06-30-10

Sheet:

Town of Mendocino Photographs: Items 33 - 41 Main Street



Mendocino County

Location:
Town of Mendocino
Main St.

ADA
Comprehensive
Access Plan

Photographs



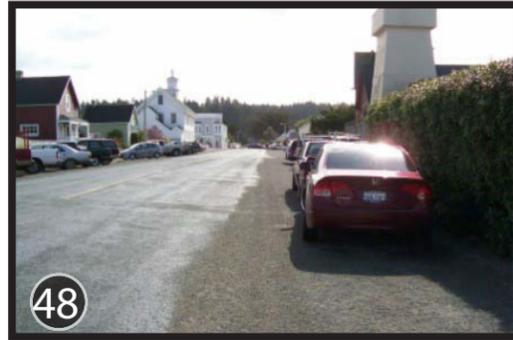
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Town of Mendocino Photographs: Items 42 - 51 Ukiah Street



Mendocino County

Location:
Town of Mendocino
Ukiah St.

ADA Comprehensive Access Plan

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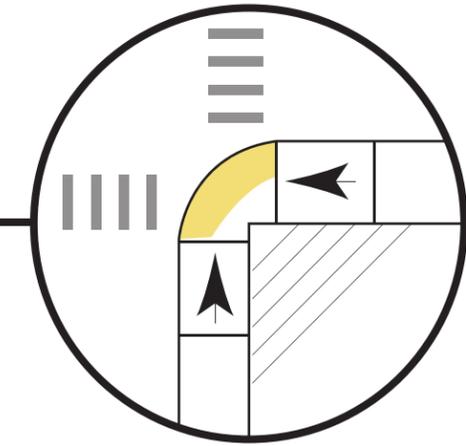
Date:
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Potter Valley

Overview

Category 3



This rural town is situated in an agricultural valley within commuting distance to Ukiah and Clear Lake areas. It is urban density for considering road right of way development along Main Street from Eel River Road to West Side Potter Valley Road. The town area is primarily housing on tract size parcels with some commercial businesses and a few civic facilities.

Terrain in the urban density area is predominately flat to gentle slopes with moderate steepness at the East Fork Russian River. From the river bridge westbound, Main Street is fully developed with traditional curb gutter and sidewalk stopping about 50 feet short of the bridge. The area east of the bridge is partly developed with drainage carried in road side ditches or swales and partial sidewalks between drainage and right of way boundary. Most all existing pedestrian sidewalk infrastructure is aged and near end of useful service life.

Commercial business is declining in the community area. A few have recently closed and some older automotive related facilities remain vacant, some appear to have been closed for many decades. Current land use zoning will afford opportunities over time to redevelop and develop commercial facilities and residential projects in the urban density area. Connections to pedestrian travel paths serving new and altered facilities should be done with the projects as they develop.



Mendocino
County

Location:

Potter Valley

CalTrans Map & Year:

3G44 1994

ADA
Comprehensive
Access Plan

Overview



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Potter Valley

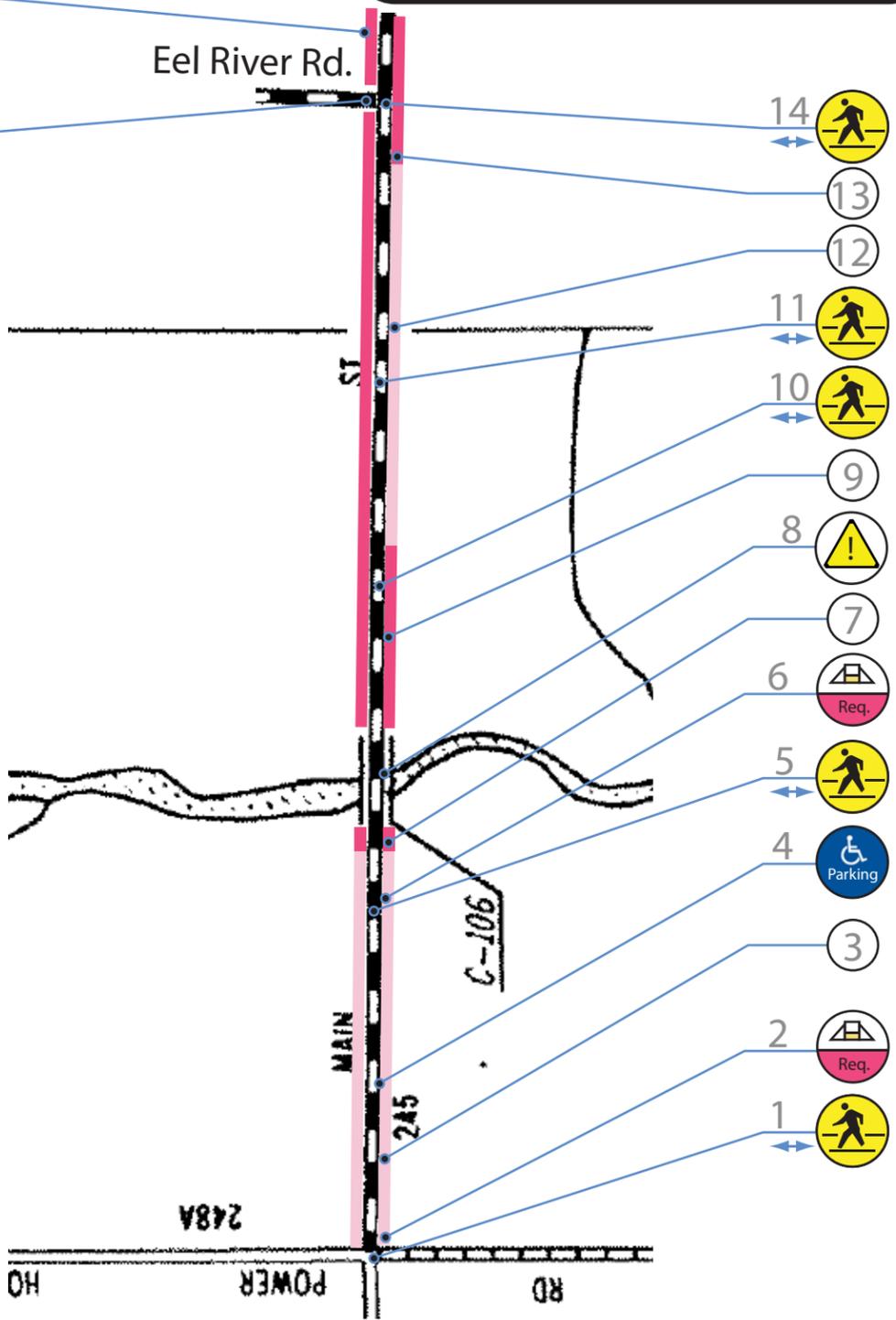
Site Description:
An urban density rural community situated each side of a county road with surrounding agricultural and large parcel residential uses.

Site Visit Pictures & Notes:

Category 3
15

16

Eel River Rd.



- 1: Crossing; recommend striping for pedestrian safety. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 2: Curb ramp or blended transition is missing at this corner and is necessary to provide continuous travel path for disabled persons.
- 3: Sidewalk; general condition along this stretch is numerous cracks, excessive cross slopes in scattered locations, excessive cross slopes at driveways, constricted widths due to vegetation and other obstructions.
- 4: On street parking is designated for disabled use by blue curb and signage. The layout does not afford unloading spaces at vehicle ground level and the road crown exceeds 2%.
- 5: Existing crosswalk; recommend heavy bar striping as this is a mid block crossing designated for children. This existing crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 6: Curb ramp or blended transition required at this location for pedestrian crossing
- 7: Sidewalk is not continuous in this area, options may include asphalt if properly sloped and connected to existing concrete.
- 8: Bridge; there is just enough width for minimum vehicle lanes at this time. Options to afford pedestrian crossing could include: one lane for vehicles, slower bridge speed (15mph/) with narrower vehicle lanes, pedestrian walk cantilevered from existing bridge, or pedestrian bridge.
- 9: Sidewalk is not continuous in this area, options may include asphalt if properly sloped and connected to existing concrete.
- 10 & 11: Existing crosswalks; recommend heavy bar striping as this is a mid block crossing designated for children. These existing crosswalks should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 12: Sidewalk; general condition along this stretch is numerous cracks, excessive cross slopes in scattered locations, excessive cross slopes at driveways, constricted widths due to vegetation and other obstructions.
- 13: Sidewalk is not continuous in this area, options may include asphalt if properly sloped and connected to existing concrete.
- 14: Crossing; considering the business activity and weekend intensive traffic due to recreation destinations nearby, there should be traffic calming and a crosswalk for pedestrian safety. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Paving is quite rough in this intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.
- 15: This corner is not developed with traditional walks or gutters. Vehicle entry lanes and provisions for pedestrian walks are not clearly recognizable. It is recommended to provide durable high contrast striping to define the walk area and vehicle entry/exit lanes to enhance pedestrian safety. Paving needs overlaid to afford a relatively smooth surface for pedestrian and disabled compliant travel path. Surface water drainage must be accommodated as well.
- 16: Crosswalk; considering the business activity and weekend intensive traffic due to recreation destinations nearby, there should be traffic calming and a crosswalk for pedestrian safety. Heavy bar striping recommended with high visibility sign paddles. Paving is quite rough in this intersection and needs repair to afford disabled compliant and safe footing. Surface slopes are suitable for a blended transition from walk to street travel.



Mendocino County

Location:
Potter Valley

CalTrans Map & Year:
3G44 1994

ADA
Comprehensive
Access Plan

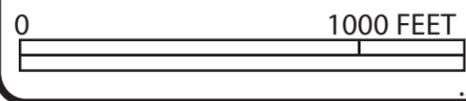


Project #:
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Date:
05-28-10

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FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Potter Valley: Items 1 - 20



Mendocino County

Location:
Potter Valley

ADA
Comprehensive
Access Plan

Photographs



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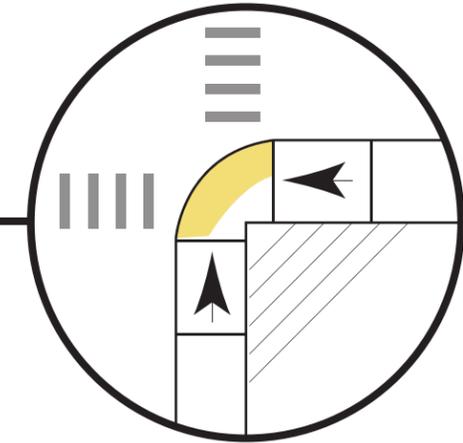
Date:
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Redwood Valley

Overview

Category 2



This small community is generally rural density for considering road right of way development. The town area is primarily scattered housing on large parcels, widely separated commercial locations, and the recognizable town having some commercial and civic facilities clustered at East Road and School Way intersection. Redwood Valley is a short commute to Ukiah and a desired residential location.

There are commercial facilities along Highway 101 frontage road, north of Highway 20, situated on large parcels with upslope terrain from the road right of way making pedestrian improvements impractical. The commercial facilities along 101 are best served by on site features for disabled access. Another small commercial area is at East Road and Road A with the site terrain significantly higher than the road. This small area is best served by on site facilities as there is no adjacent or nearby businesses or urban density area. The urban density area for commercial and civic facilities is at the tee intersection of East Road and School Way.

Terrain in the urban density area is gentle to moderate slopes with the east side of East Road trending upward creating difficulty for pedestrian travel path along the right of way or on a site. School Way is gently sloped down from East Road and then steep east of the small shopping center. Pedestrian travel paths are achievable around the small shopping center. The east side of East Road is difficult terrain slope for pedestrian travel path and has vehicle parking nose in to buildings causing traffic interference with travel path along the right of way.

Current land use zoning will afford opportunities over time to develop more commercial and industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:

Redwood
Valley

CalTrans Map & Year:

3G43 1994

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Comprehensive
Access Plan

Overview



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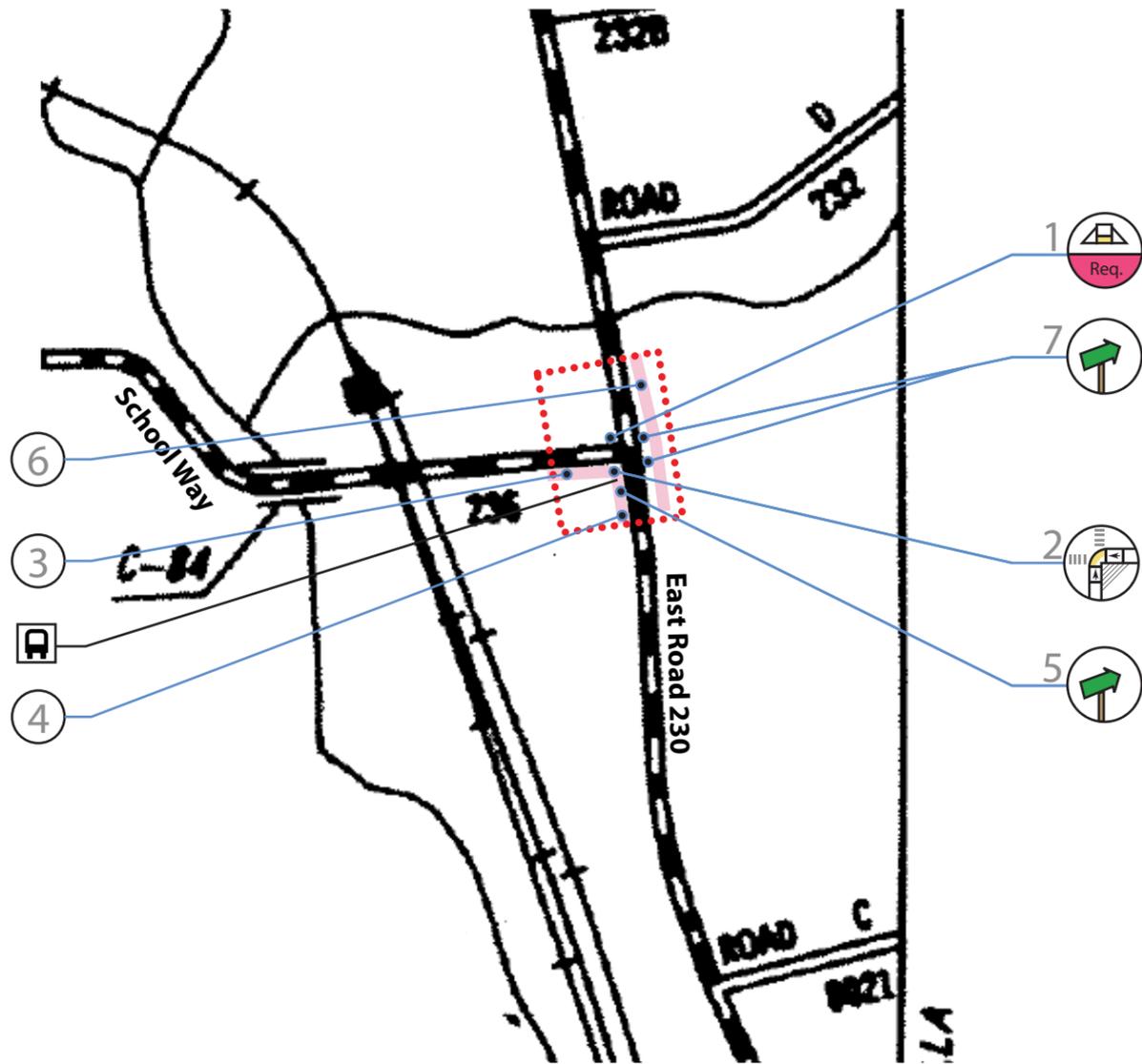
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Red wood Valley

Category 2

Site Description:
A rural commuter community spread across a rather large area with a small commercial urban density core at the intersection of East Road and School Way.



Site Visit Pictures & Notes:



1: This northwest corner site is currently undeveloped. The pedestrian travel paths should be constructed when the property is developed.



2: This corner is currently developed and in deteriorated condition for disabled access and for general pedestrian safety. Corrective work should be done to provide safe pedestrian surface conditions. This corner is also a designated MTA bus stop, travel path conditions from the actual bus stop location need to be verified for code compliance as there is no recognizable signage for the bus stop.



3: Pedestrian travel path is on asphalt with undefined boundaries for travel direction. There are no designated entry lanes for vehicles. Generally surfaces are smooth enough for disabled access compliance and some areas may have cross slope exceeding 2%. Minor paving corrections will make the walk area code compliant. It is recommended to provide durable high contrast striping to define the walk area and vehicle entry/exit lanes to enhance pedestrian safety. Travel path should end before the walkway running slope exceeds 5%. Walkway should also afford opportunity for the property owner to provide a connecting route to the building from the right of way.



4: Pedestrian travel path is on asphalt with undefined boundaries for travel direction. There are no designated entry lanes for vehicles. Generally surfaces are smooth enough for disabled access compliance and some areas may have cross slope exceeding 2%. Minor paving corrections will make the walk area code compliant. It is recommended to provide durable high contrast striping to define the walk area and vehicle entry/exit lanes to enhance pedestrian safety. Since parking is nose in to building on the south building wing, the pedestrian travel path should stop before the nose in parking and have opportunity for the property owner to provide a connecting route to the building from the right of way.



5: Since there are no sidewalks or curb ramps at intersection northwest, provide signage directing mobility impaired persons to cross west bound at this corner.



6: Pedestrian travel path is on asphalt with undefined boundaries for travel direction. There are no designated vehicle entry lanes to adjacent properties with parking on site. South of School Way beyond the commercial building, the travel path is on East Road east side road shoulder. The asphalt condition is not smooth enough for disabled travel and needs at least resurfacing to fill "alligators" and provide smooth transition at numerous ledges between paving patches. There are significant areas that have cross slopes exceeding 2% and abrupt surface level changes requiring paving modifications. Current surface water sheet flow drainage from adjacent properties is into shallow paved swales that are in the most available pedestrian path requiring careful design for pedestrian safety from the gas station at north to the commercial building at south. It is recommended to provide durable high contrast striping to define the walk area and vehicle entry/exit lanes to enhance pedestrian safety. The pedestrian route should end at the southerly commercial building as road shoulder widening is necessary for continuing to the bank building and is considered technically infeasible at this time.



7: Since the northwest corner is currently undeveloped, provide signage at the northerly and southerly intersection cross walks directing mobility impaired persons to cross east bound to the intersection southwest corner.



Mendocino County

Location:

Redwood Valley

CalTrans Map & Year:

3G43 1994

ADA Comprehensive Access Plan



Ref. North



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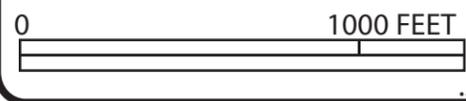
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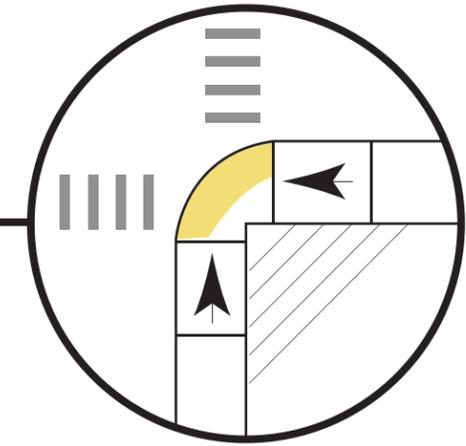
FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Talmage

Overview

Category 2



Talmage is recognizable as a distinct village within the Ukiah Valley Area Plan and within the Caltrans recognized urban area. The town area is primarily urban density housing with some commercial and civic facilities. Surrounding the town village area are farms and residential on large lots. All commercial and civic facilities are along Talmage Road (State Highway 222) from East Side Road at Sanford Ranch Road near “City of Ten Thousand Buddhas” to Sanford Ranch Road tee intersection near the Mill Creek Bridge.

Terrain in the urban density area is predominately flat with a few localized gentle slopes. Pedestrian travel paths are achievable along each side of Talmage Road.

Current and future land use zoning will afford opportunities over time to develop more commercial facilities, industrial facilities, and residential tracts in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop.



Mendocino
County

Location:

Talmage

CalTrans Map & Year:

3H14 1994

ADA
Comprehensive
Access Plan

Overview



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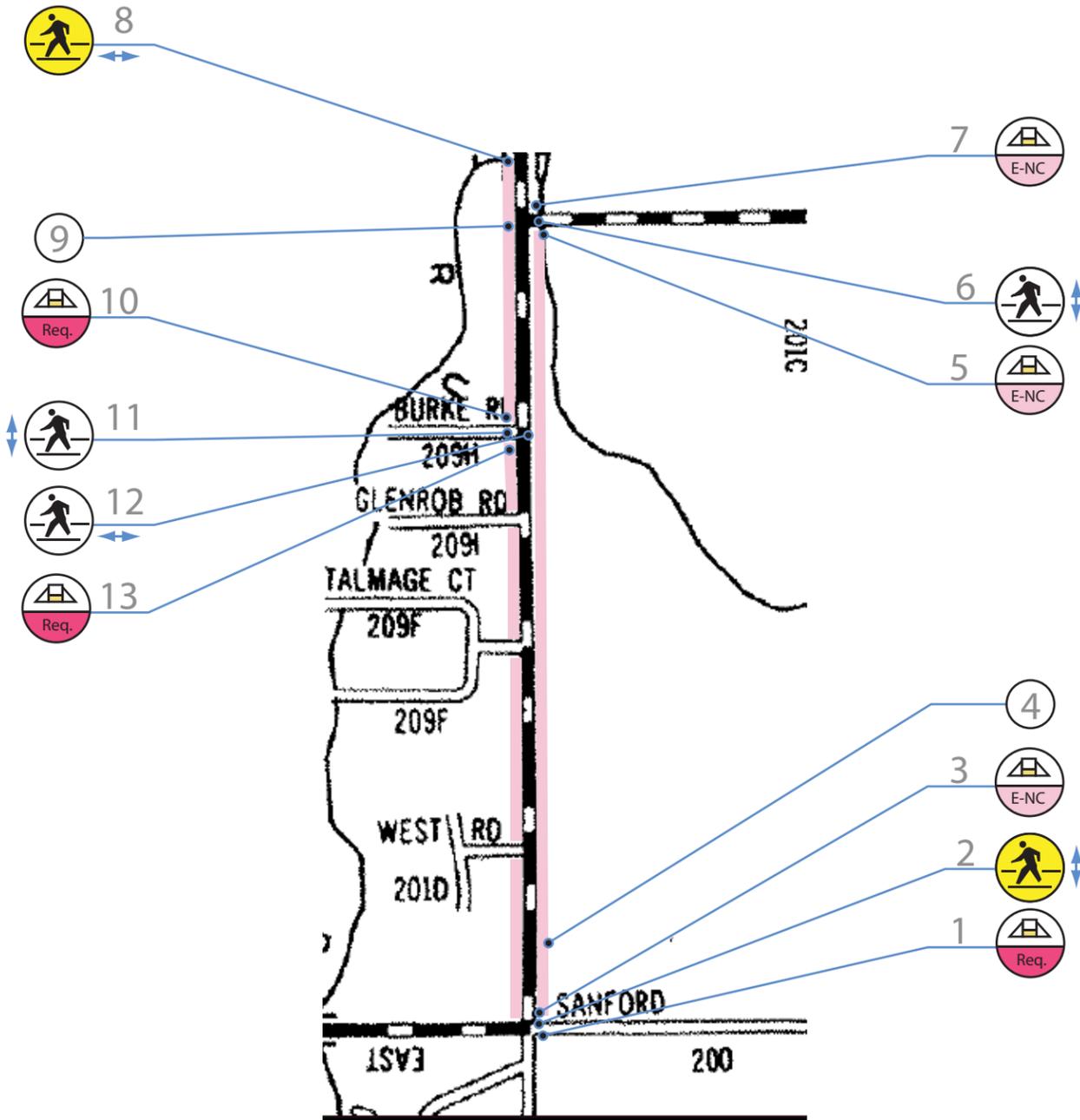
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Talmage

Category 2

Site Description:
Talmage is a rural community within an urban plan area with an urban density commercial area along Talmage Road (CA highway 222).



Site Visit Pictures & Notes:

- 1: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 2: Existing crosswalk; rough paving in patches with paving edge ledges and minor potholes; resurfacing required for disabled compliant surface.
- 3: This corner is not developed with traditional walks or gutters. Vehicle entry lanes and provisions for pedestrian walks are not clearly recognizable. It is recommended to provide durable high contrast striping to define the walk area and vehicle entry/exit lanes to enhance pedestrian safety. Paving needs overlaid to afford a relatively smooth surface for pedestrian and disabled compliant travel path. Surface water drainage must be accommodated as well.
- 4: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with vehicle parallel parking, and the available route is not fully paved, striped, or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and parking is limited or controlled. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. The alternative is to construct new traditional style curbs gutters and sidewalks along the road right of way edge requiring significant design and construction expense.
- 5: Curb ramp; cross slope on approach and top landing exceed 2% max, ledge at ramp bottom to gutter, no detectable warnings, run slope at gutter exceeds 5% max; reconstruction required.
- 6: Crossing: striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Pavement is rough in places and should be overlaid to provide proper cross slope and smooth pedestrian and disabled compliant travel path.
- 7: Curb ramp; cross slope on approach and top landing exceed 2% max, ledge at ramp bottom to gutter, no detectable warnings, run slope at gutter exceeds 5% max; reconstruction required.
- 8: Crosswalk recommended for connection to commercial and civic facilities across the road. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 9: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with vehicle parallel parking, and the available route is not fully paved, striped, or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and parking is limited or controlled. There may be sufficient width to develop a separated walk adjacent to property frontages on this side of the road. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
- 10: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 11: Crossing: striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Pavement is rough in places and should be overlaid to provide proper cross slope and smooth pedestrian and disabled compliant travel path.
- 12: Crosswalk recommended for connection to commercial and civic facilities across the road. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 13: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.



Mendocino County

Location:

Talmage

CalTrans Map & Year:

3H14 1994

ADA Comprehensive Access Plan



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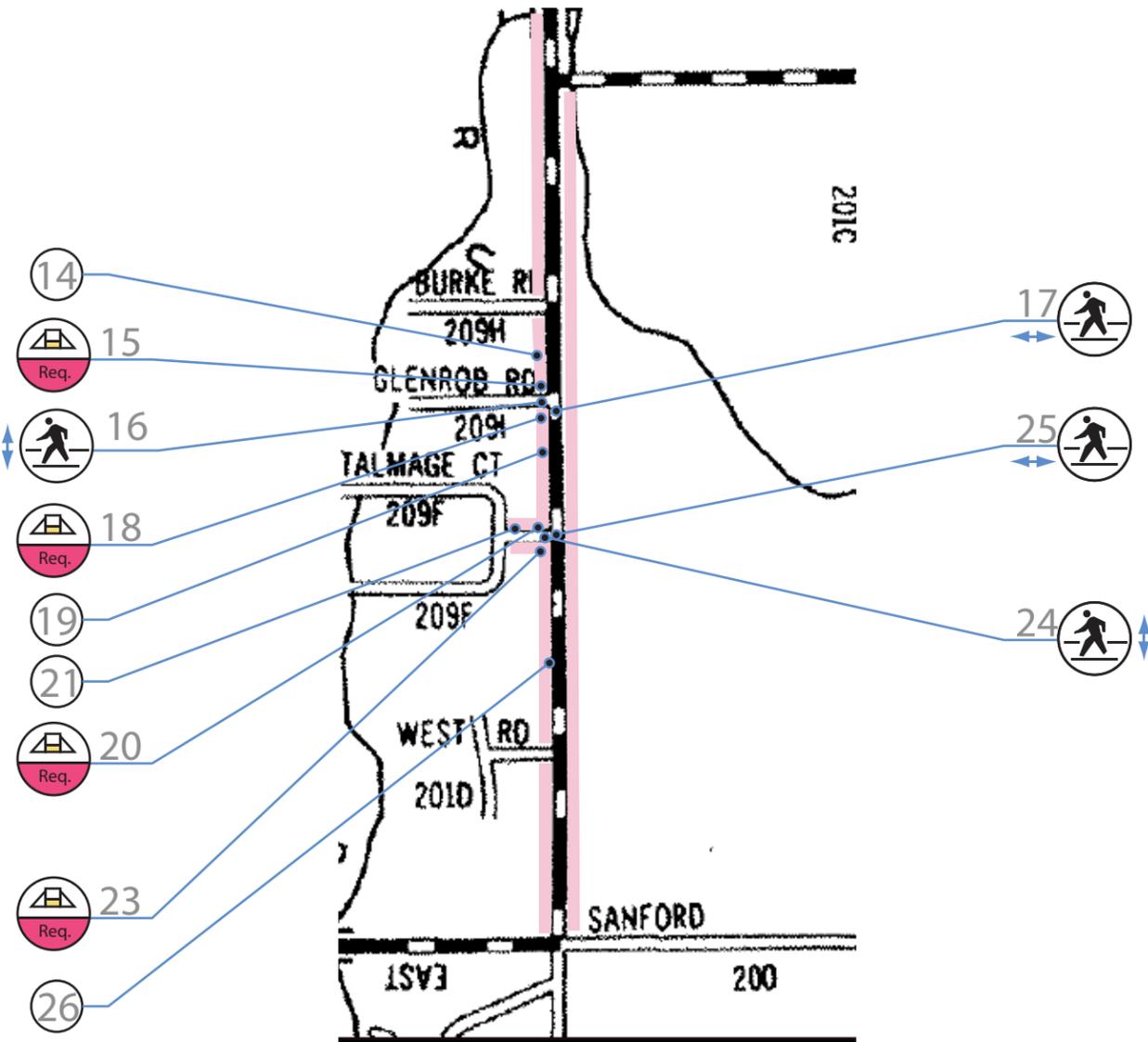
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OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————
MINOR ARTERIAL	—————	MINOR ARTERIAL	—————	MINOR ARTERIAL	—————
COLLECTOR	—————	MAJOR COLLECTOR	—————	MAJOR COLLECTOR	—————
LOCAL	—————	MINOR COLLECTOR	—————	MINOR COLLECTOR	—————
LOCAL	—————	LOCAL	—————	LOCAL	—————

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
- SW: Sidewalk Not Required				

Talmage

Category 2

Site Description:
Talmage is a rural community within an urban plan area with an urban density commercial area along Talmage Road (CA highway 222).



Site Visit Pictures & Notes:

- 14: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with vehicle parallel parking, and the available route is not fully paved, striped, or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and parking is limited or controlled. There may be sufficient width to develop a separated walk adjacent to property frontages on this side of the road. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
- 15: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 16: Crossing; striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Pavement is rough in places and should be overlaid to provide proper cross slope and smooth pedestrian and disabled compliant travel path.
- 17: Crosswalk should be considered for connection to commercial and civic facilities across the road even though the block length in this area is relatively short. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 18: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 19: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with vehicle parallel parking, and the available route is not fully paved, striped, or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and parking is limited or controlled. There may be sufficient width to develop a separated walk adjacent to property frontages on this side of the road. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
- 20: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 21: Existing sidewalk is traditional concrete with rolled curb and integral gutter. Replacement necessary due to deteriorated condition and excessive cross slope at driveway aprons.
- 22: Existing sidewalk is traditional concrete with rolled curb and integral gutter. Replacement necessary due to deteriorated condition and excessive cross slope at driveway aprons.
- 23: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 24: Crossing; striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Pavement is rough in places and should be overlaid to provide proper cross slope and smooth pedestrian and disabled compliant travel path.
- 25: Crosswalk should be considered for connection to commercial and civic facilities across the road even though the block length in this area is relatively short. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 26: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with vehicle parallel parking, and the available route is not fully paved, striped, or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and parking is limited or controlled. There may be sufficient width to develop a separated walk adjacent to property frontages on this side of the road. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.



Mendocino County

Location:
Talmage

CalTrans Map & Year:
3H14 1994

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Project #:
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Date:
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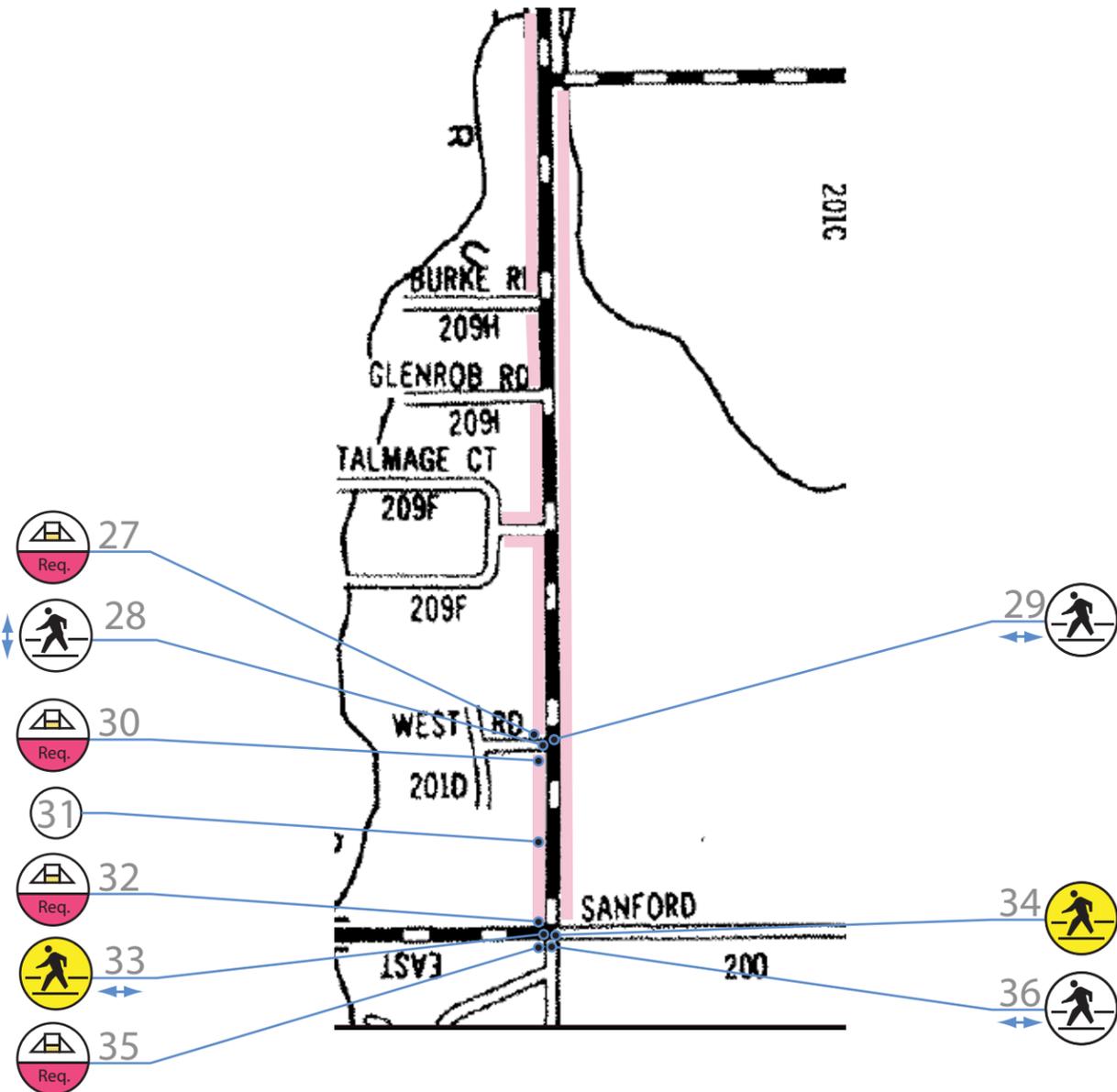
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OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————	OTHER PRINCIPAL ARTERIAL	—————
MINOR ARTERIAL	—————	MINOR ARTERIAL	—————	MINOR ARTERIAL	—————
COLLECTOR	—————	MAJOR COLLECTOR	—————	MAJOR COLLECTOR	—————
LOCAL	—————	MINOR COLLECTOR	—————	MINOR COLLECTOR	—————
LOCAL	—————	LOCAL	—————	LOCAL	—————

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
- SW: Sidewalk Not Required				

Talmage

Category 2

Site Description:
Talmage is a rural community within an urban plan area with an urban density commercial area along Talmage Road (CA highway 222).



Site Visit Pictures & Notes:

- 27: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 28: Crossing: striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Pavement is rough in places and should be overlaid to provide proper cross slope and smooth pedestrian and disabled compliant travel path.
- 29: Crosswalk should be considered for connection to commercial and civic facilities across the road even though the block length in this area is relatively short. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 30: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 31: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with vehicle parallel parking, and the available route is not fully paved, striped, or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and parking is limited or controlled. There may be sufficient width to develop a separated walk adjacent to property frontages on this side of the road. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
- 32: Blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 33: Existing crosswalk: Pavement is rough in places, has minor potholes, and should be overlaid to provide smooth pedestrian and disabled compliant travel path. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 34: Existing crosswalk: Pavement is rough in places, has minor potholes, and should be overlaid to provide smooth pedestrian and disabled compliant travel path. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 35: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 36: Crossing: Striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Pavement is rough in places, has minor potholes, and should be overlaid to provide smooth pedestrian and disabled compliant travel path.



Mendocino County

Location:

Talmage

CalTrans Map & Year:

3H14 1994

ADA Comprehensive Access Plan



Ref. North



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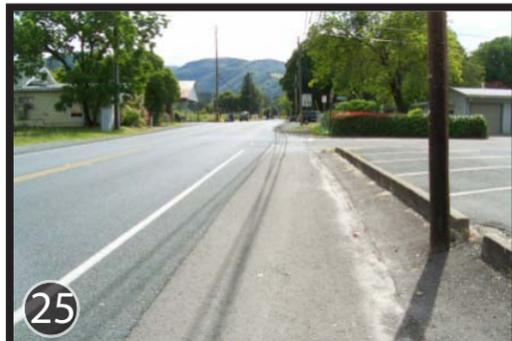
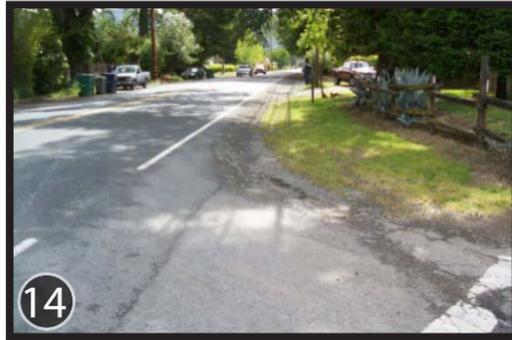
Date: 05-28-10

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FUNCTIONAL CLASSIFICATION		URBAN		RURAL	
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OTHER PRINCIPAL ARTERIAL		MINOR ARTERIAL		MAJOR COLLECTOR	
MINOR ARTERIAL COLLECTOR		MINOR COLLECTOR		MINOR COLLECTOR	
LOCAL		LOCAL		LOCAL	

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- CR: Existing Non-Compliant	- CR: Existing Compliant	- Hazard	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- CR: Existing Compliant	- Crosswalk Existing	- Crosswalk Existing	- Transit Stop	
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Crosswalk Existing	- Crosswalk Existing	- Crosswalk Existing	- Crosswalk Existing	
- SW: Sidewalk Not Required	- Crosswalk Existing	- Crosswalk Existing	- Crosswalk Existing	- Crosswalk Existing	- Crosswalk Existing	

Talmage Photographs: Items 1 - 28



Mendocino County

Location:

Talmage

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Talmage Photographs: Items 29 - 36



Mendocino
County

Location:

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Ukiah, North

Overview

Category 1

Mendocino County has jurisdiction for a densely developed area along North State Street with retail businesses, restaurants, civic facilities, tract density residential area west of North State Street, and industrial sites (beyond Ukiah City limits) to the east. Prior to US 101 freeway being constructed, State Street was part of US 101 with a number of visitor service oriented businesses. The visitor serving businesses are still being highly utilized as they are just off a freeway intersection.

The City of Ukiah limit crosses North State Street just north of Bricarelli Drive. There are some “jogs” in the City of Ukiah limits yet it appears all the road right of way is in County jurisdiction north of the City limits sign. This urban density area has fully developed pedestrian infrastructure comprised of signal intersections, traditional concrete curb, gutter, and sidewalks with scattered portions having asphalt walks. Most all of the sidewalks, crosswalks, and pedestrian signal facilities are aged with a good portion near the end of service life.

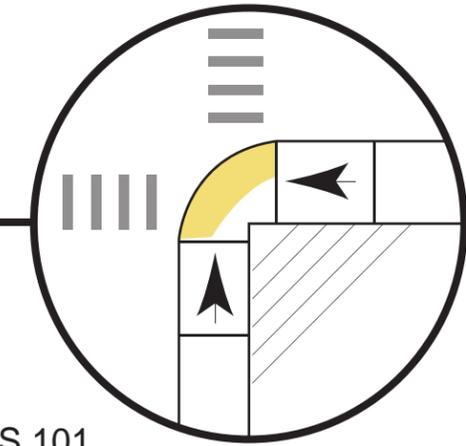
Terrain in the North State Street area is generally flat with moderate slopes in the east area and gentle slope in the west residential area.

Certain portions of the County jurisdiction will not be included in the report graphics and text as they are either undeveloped, low density, or a lower priority for urban density pedestrian infrastructure. These portions are:

- Brush Street; east of railroad tracks. This area is low density industrial with a majority not developed yet. Briggs Street, Brunner Street, and a north portion of Orr Street are gravel or unimproved.
- Ford Road; east of North State Street. This area is urban density with fully developed sidewalks. There are very low intensity uses and mostly undeveloped parcels east of the shopping center resulting in hardly any pedestrian traffic. Pedestrian travel paths need correction for disabled access code compliance, mostly due to excessive cross slope at driveway aprons.
- County jurisdiction residential area west of North State Street and north of city limits. The area has mostly full developed pedestrian infrastructure with localized gaps in sidewalks primarily in the older developments. It appears a majority of the residential development is from the 1970's and 1980's. Most sidewalks and curb ramps are not disabled code compliant. The highest priority that could be considered is connecting the neighborhood bus stops to North State St.

Current land use zoning will afford opportunities over time to intensify commercial sites, industrial facilities, and residential tracts in the urban density area. Connections or upgrades to existing pedestrian travel paths serving new or intensified facilities should be done with the projects as they develop.

Future regional land use planning processes underway will most likely have urban density north of the current rural to urban limit. It is recommended that the County be actively involved with the infrastructure planning portion of the Ukiah Valley Area Plan for policies and funding mechanisms.



Mendocino County

Location:

Ukiah, North

CalTrans Map & Year:

3G53 1994

ADA Comprehensive Access Plan

Overview



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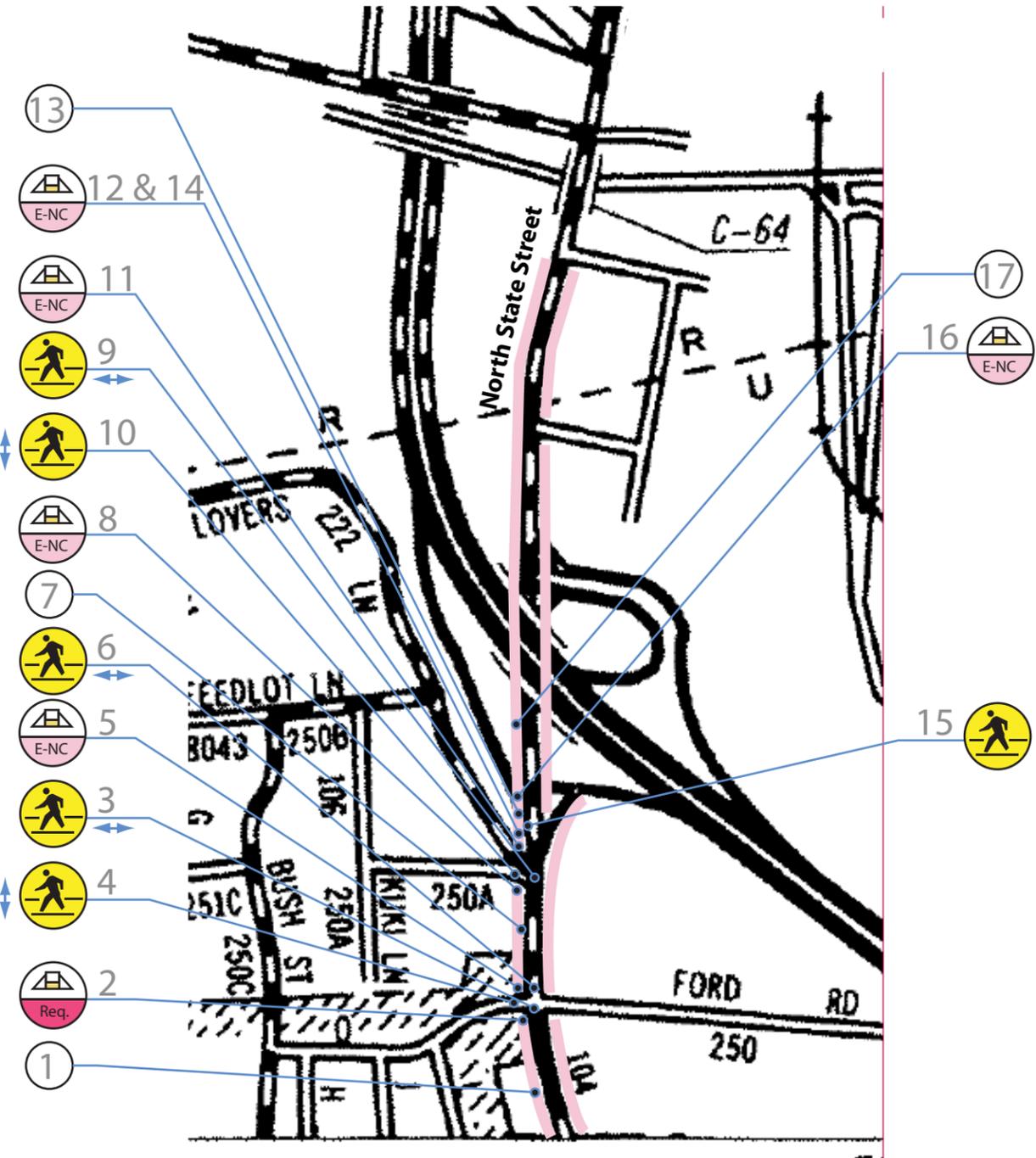
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Ukiah, North

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, apartment density residential, and residential tracts; that adjoins an incorporated city.



Site Visit Pictures & Notes:

- 1: This sidewalk is asphalt along the block corner to corner. City limit is just north of Bricarelli Drive corner. The complete block needs disabled compliant sidewalk paving due to excessive cross slopes on the walking surfaces and at drive aprons. Many vehicles are parked in the walkway area, parking controls should be determined. If the walkway will remain on asphalt, there should be striping to clearly delineate the walkway area. The only available pedestrian crossing in County jurisdiction is quite a distance away at Ford Road. The City of Ukiah should be contacted to cooperate with upgrading the Bricarelli corner with a curb ramp, smoothing pavement at crosswalks, and correcting localized crowns exceeding 5%.
- 2: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path. Underground infrastructure coordination required to upgrade this corner.
- 3: Existing crosswalk; rough paving, paving ledges, excessive gutter slope, excessive crown running slope, and striping needs aligned to afford curb ramp maneuvering turns.
- 4: Existing crosswalk; rough paving in patches, excessive cross slope, resurfacing required for disabled compliant surface. Striping needs aligned to afford curb ramp maneuvering turns.
- 5: Curb ramp; wood (power, communications) pole interferes with required top landing size, excessive gutter slope, no detectable warnings. Reconstruction required.
- 6: Existing crosswalk; rough paving, paving ledges, excessive gutter slope, excessive crown running slope, and striping needs aligned to afford curb ramp maneuvering turns.
- 7: Sidewalk; excessive cross slopes at driveways and slippery surface at leaking fire hydrant. Reconstruction required.
- 8: Curb ramp; inadequate top landing size, excessive gutter slope, excessive steep side flares, no detectable warnings. Reconstruction required.
- 9 & 10: Existing crosswalk; rough paving, paving ledges, excessive gutter slope, excessive crown running slope, and striping needs aligned to afford curb ramp maneuvering turns
- 11 & 12: Curb ramp; no top landing, excessive ramp slope, excessive gutter slope, excessive steep side flares, no detectable warnings. Reconstruction required.
- 13: Sidewalk; width obstructions due to pole and side vegetation. Excessive cross slope for most of the sidewalk length, storm water drain plate is bent, hazard for wheelchairs and pedestrians.
- 14: Curb ramp, drop style; debris collecting in bottom landing area, need better drainage design. Lip from landing to gutter. Corrections required.
- 15: Existing crosswalk; due to traffic intensity, heavy bar striping recommended .
- 16: Curb ramp; ponding water at gutter area, corrective work required. No detectable warnings.
- 17: Sidewalk; excessive cross slope in scattered locations, restricted width due to vegetation, excessive cross slope at driveways, scattered ledges exceeding 1/4".



Mendocino County

Location:

Ukiah, North

CalTrans Map & Year:

3G53 1994

ADA Comprehensive Access Plan



Ref. North

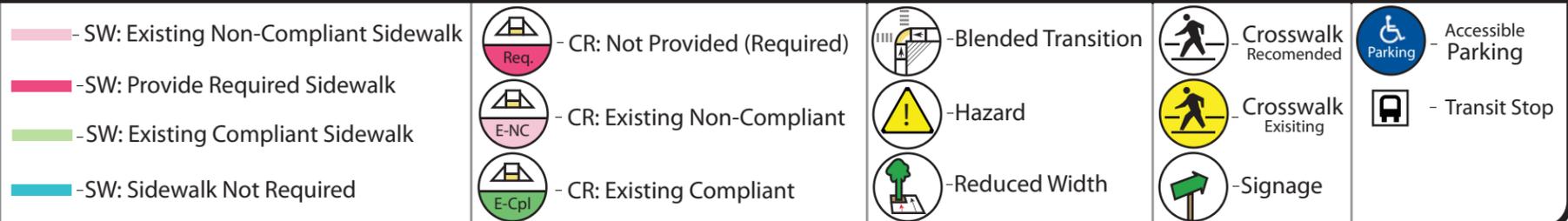
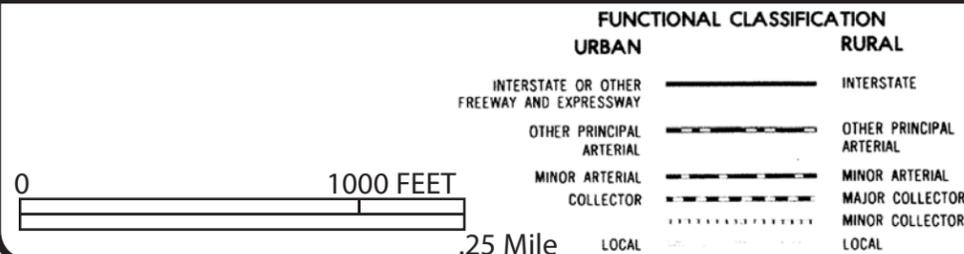


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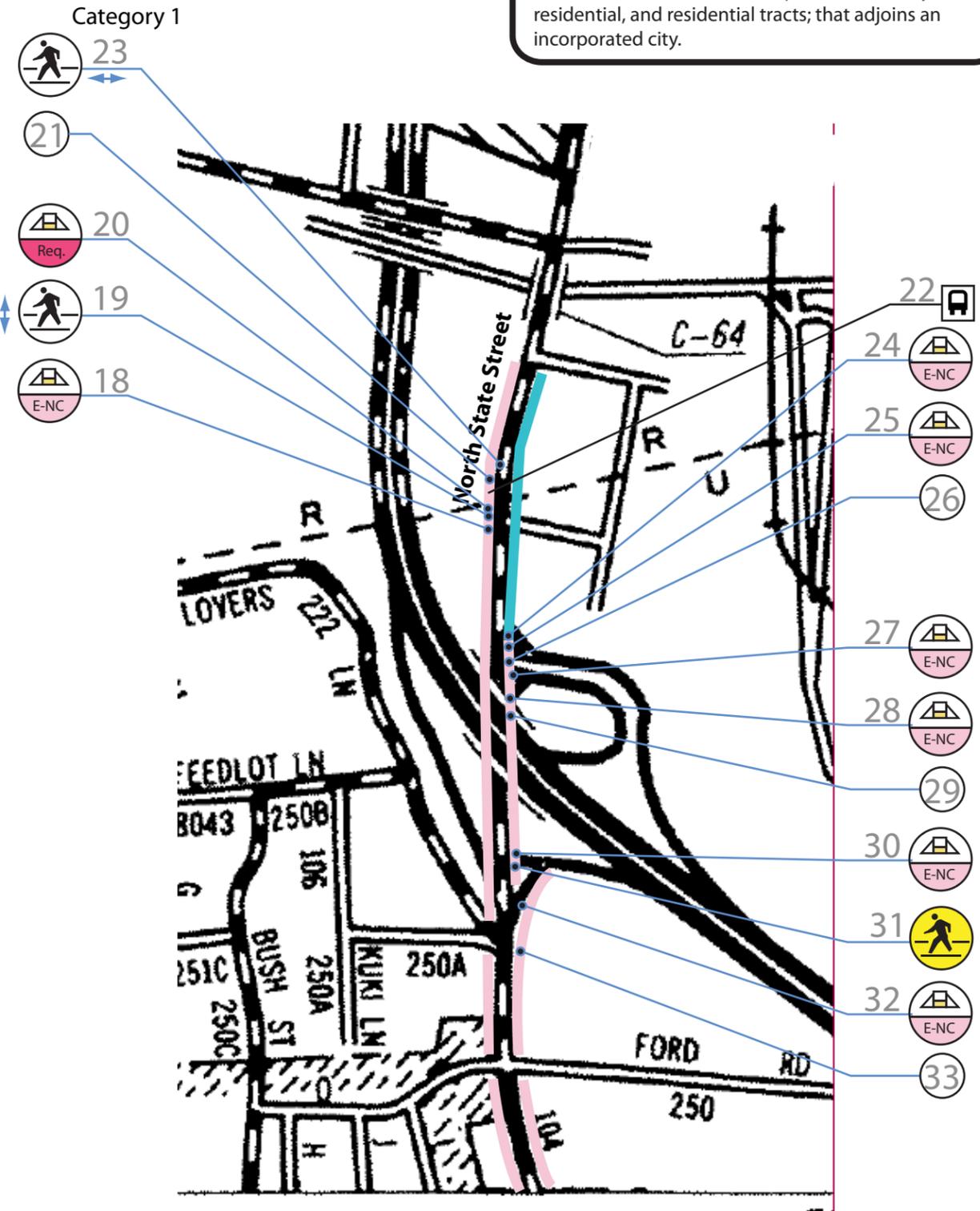
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Ukiah, North

Site Description:
An unincorporated urban density area with commercial, industrial, civic, apartment density residential, and residential tracts; that adjoins an incorporated city.

Site Visit Pictures & Notes:



- 18: Curb ramp; no side protection to keep wheelchair from dropping off curb in travel direction, no detectable warnings, excessive gutter run slope, ramp bottom lip at gutter. Reconstruction required.
- 19: Crossing; excessive rough, paving correction required. Recommend striping.
- 20: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 21: Sidewalk; excessive cross slope at driveways and scattered dislodged sections with ledges.
- 22: Busstop; insufficient width perpendicular to road direction for bus to deploy a lift.
- 23: Crosswalk recommended in this area to serve the recently installed bus stop. Most developed sites and businesses are across the street. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 24: Curb ramp; ponding water at gutter area, corrective work required. Debris should be cleaned to afford sure footing.
- 25: Curb ramp; ramp bottom lip at gutter, no detectable warnings, full size top landing not available due to angle layout. Reconstruction required.
- 26: Sidewalk; excessive cross slope for most of length.
- 27: Curb ramp; ponding water at gutter area, corrective work required. Debris should be cleaned to afford sure footing.
- 28: Curb ramp; ponding water at gutter area, gutter lip at landing, corrective work required. Debris should be cleaned to afford sure footing.
- 29: Sidewalk; excessive cross slope in scattered locations, ledges in scattered locations, vault and manhole lids have ridges and recesses exceeding 1/4".
- 30: Curb ramp; gutter lip at landing, corrective work required.
- 31: Existing crosswalk; heavy bar striping recommended due to traffic intensity, detectable warnings missing at lane island, surface very rough in lane island area
- 32: Curb ramp; ponding water at gutter area, gutter lip at landing, corrective work required. Debris should be cleaned to afford sure footing.
- 33: Sidewalk; excessive cross slope in scattered locations, ledges in scattered locations, vault and manhole lids have ridges and recesses exceeding 1/4", storm water drain plate is bent, hazard for wheelchairs and pedestrians.



Mendocino County

Location:
Ukiah, North

CalTrans Map & Year:
3G53 1994

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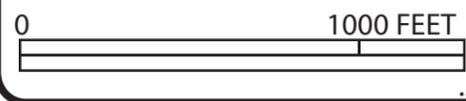
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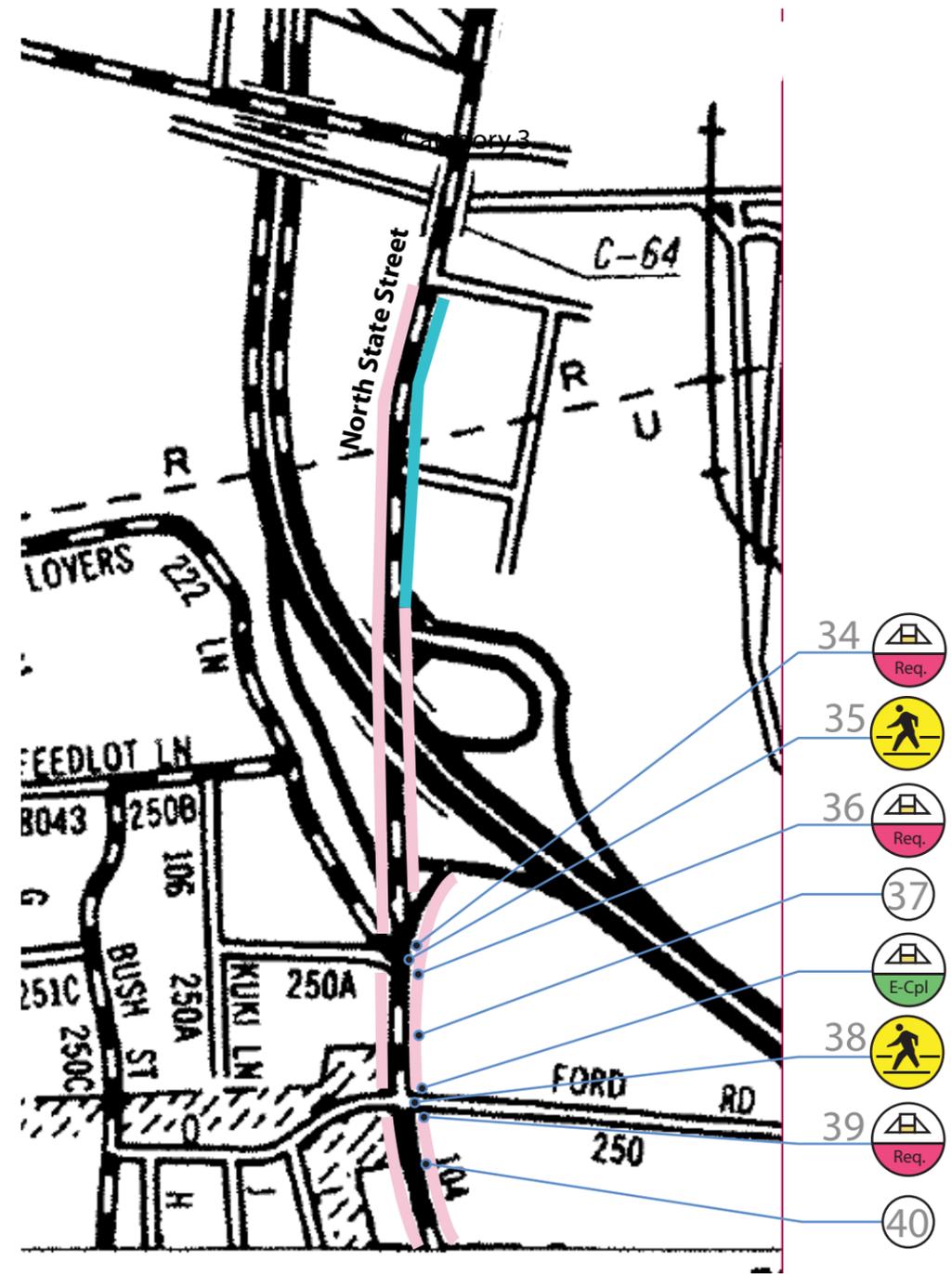
FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Ukiah, North

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, apartment density residential, and residential tracts; that adjoins an incorporated city.



Site Visit Pictures & Notes:

- 34: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path. Underground infrastructure coordination required to upgrade this corner.
- 35: Existing crosswalk; rough paving, striping faded, need integrated to curb ramp installation.
- 36: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path. Nearby existing curb ramp is non compliant and is close enough to this corner to require an integrated design for pedestrian crossings. Underground infrastructure coordination required to upgrade this corner.
- 37: Sidewalk; asphalt and concrete sections are in poor condition and need replaced for safety and disabled access.
- 38: Existing crosswalk; rough paving, striping fading, excessive crown run slope, striping not aligned for curb ramp bottom maneuvering.
- 39: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path. Underground infrastructure coordination required to upgrade this corner.
- 40: Sidewalk; excessive cross slope in scattered locations and at driveways, some scattered ledges that can be ground flush.



Mendocino County

Location:
Ukiah, North

CalTrans Map & Year:
3G53 1994

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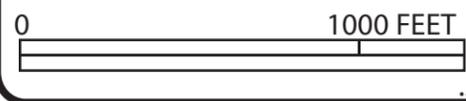


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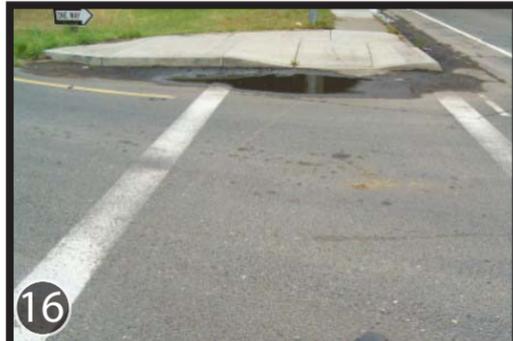
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<p>FUNCTIONAL CLASSIFICATION</p> <p>URBAN</p> <p>INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY</p> <p>OTHER PRINCIPAL ARTERIAL</p> <p>MINOR ARTERIAL</p> <p>COLLECTOR</p> <p>LOCAL</p>	<p>RURAL</p> <p>INTERSTATE</p> <p>OTHER PRINCIPAL ARTERIAL</p> <p>MINOR ARTERIAL</p> <p>MAJOR COLLECTOR</p> <p>MINOR COLLECTOR</p> <p>LOCAL</p>	- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
		- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
		- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
		- SW: Sidewalk Not Required				



Ukiah, North Photographs: Items 1 - 25



Mendocino County

Location:

Ukiah, North

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Ukiah, North Photographs: Items 26 - 40



Mendocino County

Location:

Ukiah, North

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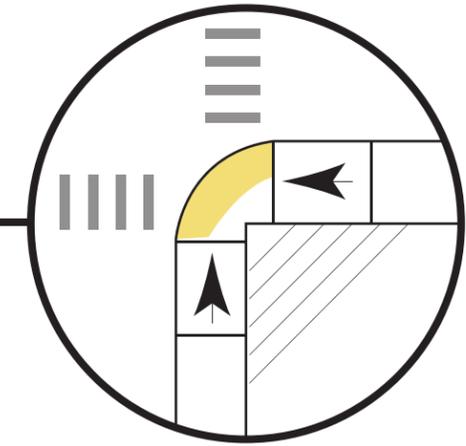
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Ukiah, South



Overview

Category 1

Mendocino County has jurisdiction for a relatively large urban density area (beyond Ukia City limits) along South State Street and west encompassing businesses, civic facilities, higher density residential near or adjacent to South State Street, and residential tracts to the west. Prior to US 101 freeway being constructed, State Street served as part of US 101 and has a number of businesses oriented to visitor services that currently are in decline or redeveloped to other uses. The City of Ukiah limit crosses South State Street aligned along the northerly side of Beacon Lane then proceeds south along the airport with all of South State Street right of way in County jurisdiction. The urban density area should have fully developed pedestrian and disabled compliant travel paths in the street right of way comprising a complete system.

General terrain in the area is relatively flat with gentle slopes downward southbound. There are localized areas of moderate slopes along the roadway making pedestrian improvement design a bit difficult for compliant disabled access. Road right of way width appears to be sufficient for 2 lanes each way with center turn lanes and possibly parallel parking or bikeways.

The west residential areas were not characterized in detail for this report and can be best described as urban, low density tracts with custom homes on the sloping hillsides and south of the creek. Pedestrian improvements in the residential streets right of way are aging, do not have continuous travel paths, and are considered relatively low priority for upgrades at this time.

Land use zoning, both current and proposed as part of the Ukiah Valley Area Plan, will afford opportunities over time to develop more commercial and industrial facilities in the urban density area. Connections to pedestrian travel paths serving new facilities should be done with the projects as they develop. Future development in this area may require additional street vehicle lanes and signals at selected intersections; these features should be planned prior to development being intensified with adequate pedestrian travel paths integrated to the overall future designs. Cooperation with the City of Ukiah will be necessary to provide useable connections to the City's right of way pedestrian facilities.



Mendocino County

Location:

Ukiah, South

CalTrans Map & Year:
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&
3H14 1994

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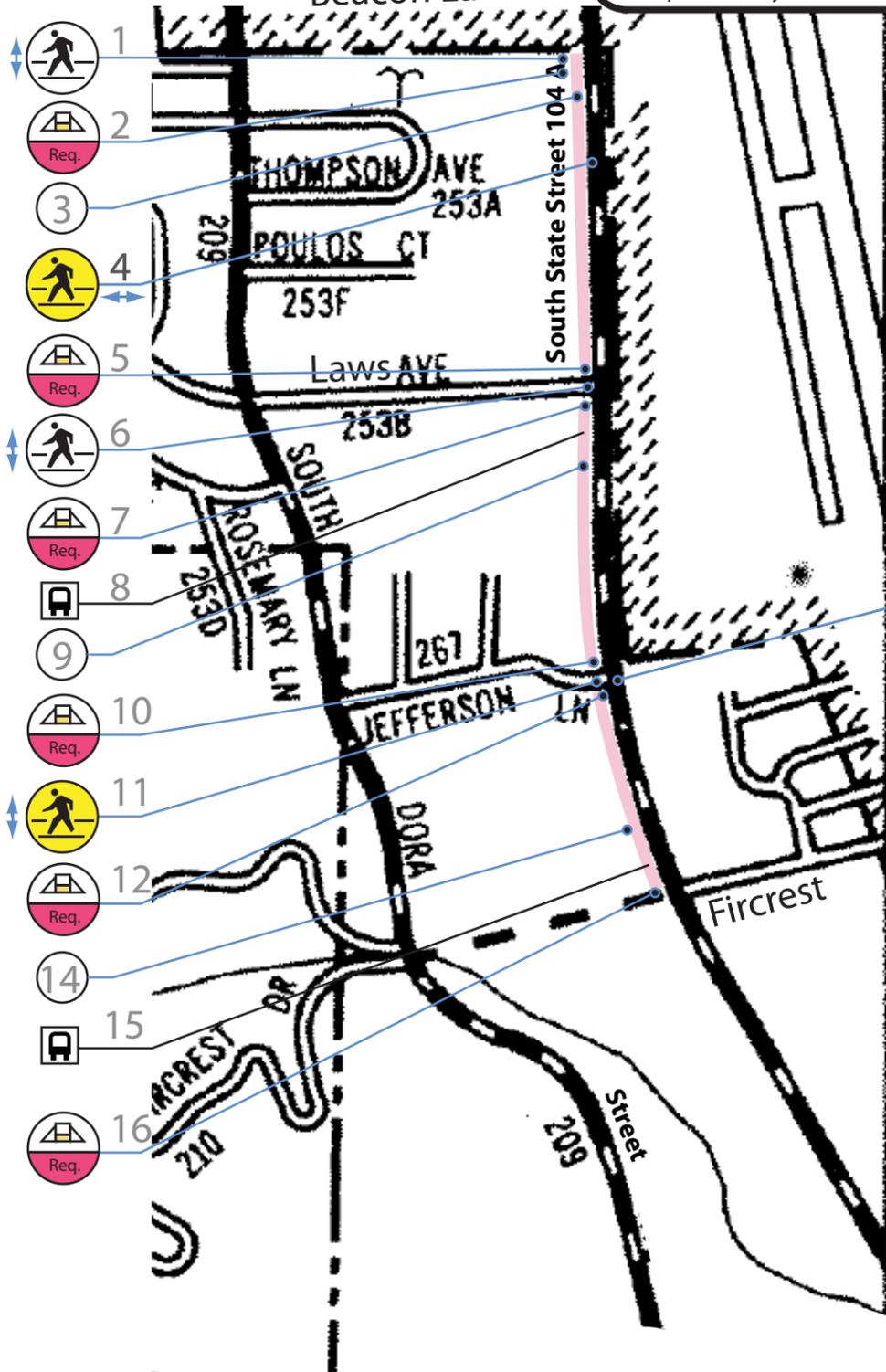
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Ukiah, South

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, higher density residential uses, and low density residential tracts; adjoining an incorporated city.



Site Visit Pictures & Notes:

- 1: Crossing; striping recommended in alignment with City curb ramp at northwest of this tee intersection. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 2: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 3: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. The alternative is to construct new traditional style curbs gutters and sidewalks along the road right of way edge requiring significant design and construction expense.
- 4: Existing crosswalk; (aligned with Airport business entry road), rough paving in patches with paving edge ledges and minor potholes; resurfacing required for disabled compliant surface. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 5: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 6: Crosswalk; striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 7: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 8: Bus stop; no provisions for disabled loading, unloading, or connection to compliant travel path; paving improvements required.
- 9: Sidewalk; from Laws Ave to Jefferson Lane: a majority of this road stretch can be improved with sidewalks adjoining the right of way edge. There will be challenges to design and construct a pedestrian walk around and over the creek channel.
- 10: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 11: Existing crosswalk; rough paving in patches with paving edge ledges and minor potholes; resurfacing required for disabled compliant surface. Striping is faded and should be repainted.
- 12: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 13: Existing crosswalk; (aligned at south of this tee intersection), slopes exceeding 5%, rough paving in patches, paving edge ledges, and minor potholes; resurfacing required for disabled compliant surface. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 14: Sidewalk; this section is fully developed with curb gutter and sidewalk. Reconstruction is necessary to afford compliant 2% max cross slopes at driveways, replace manhole covers or remove large pattern grooves, localized joints and cracks with ledges, and near Fircrest a localized area with steep run slope.
- 15: Bus stop; no provisions for disabled loading, unloading, or waiting adjacent to bench; paving corrections required.
- 16: Curb ramp; debris may cause slippery footing for pedestrians, adjacent property Owner should provide cleanup and debris containment near sidewalk edge. Water ponds at ramp gutter, drainage flow needs corrected.



Mendocino County

Location:
Ukiah, South

CalTrans Map & Year:
3H13 1994

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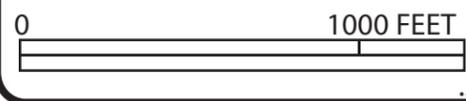


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Date:
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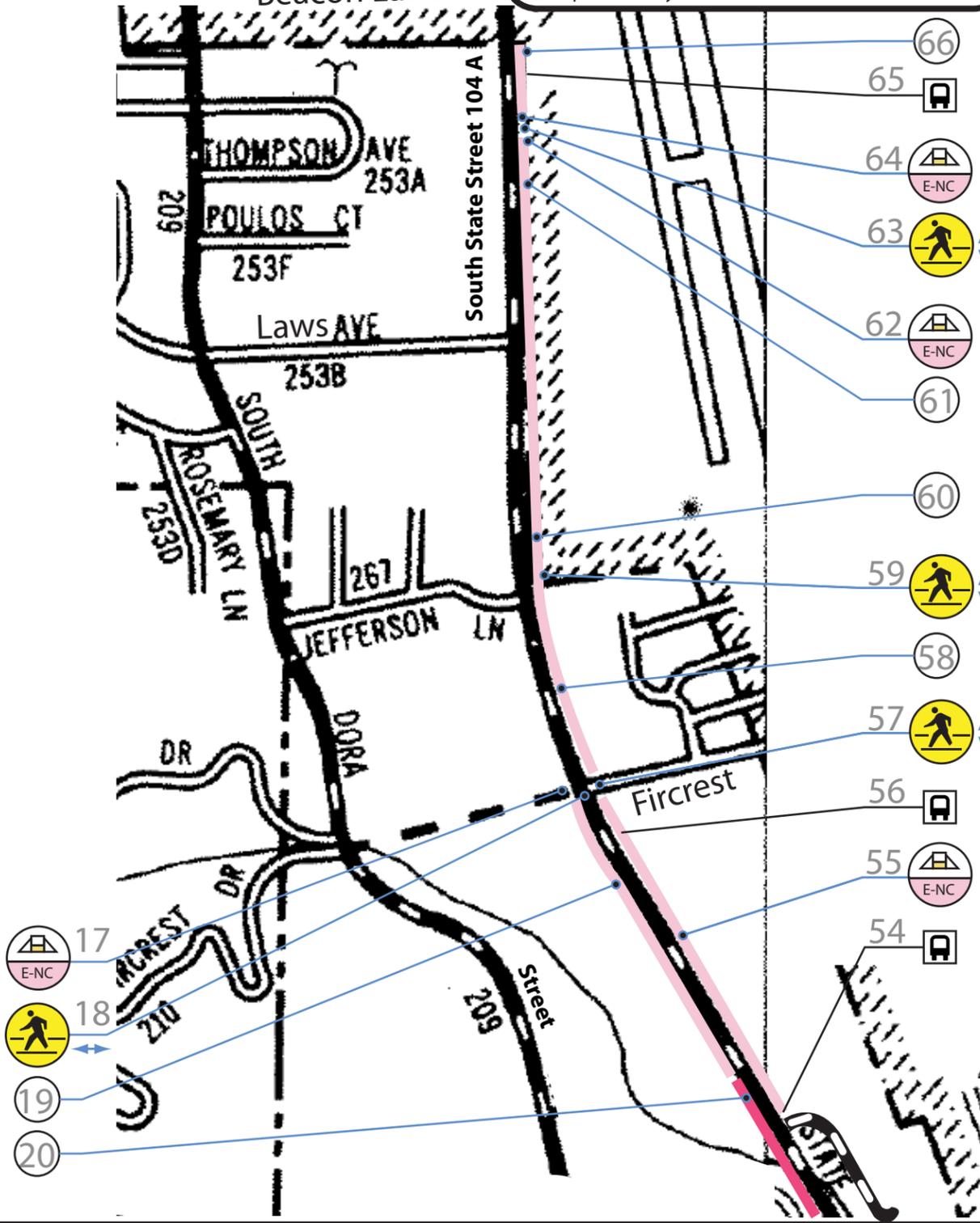
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Ukiah, South

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, higher density residential uses, and low density residential tracts; adjoining an incorporated city.



Site Visit Pictures & Notes:

- 17: Curb ramp; water ponds at ramp gutter and into crosswalk, mud deposits in standing water can cause slippery footing; drainage flow needs corrected.
- 18: Existing crosswalk; due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
- 19: Sidewalk; this area is undeveloped and needs a sidewalk to provide a connected and useable pedestrian disabled compliant travel path. If there is sufficient width for pedestrians and bicycles, the travel path could be developed along the road shoulder.
- 20: Sidewalk; this section is near fully developed with curb gutter and sidewalk to within about five hundred feet of Oak Knoll Road. Most of the sidewalk is compliant except a few driveway aprons.
- 54: Bus stop; no provisions for disabled waiting adjacent to bench; paving corrections required.
- 55: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. Sidewalk should extend from the MTA bus stop north bound to Blue Bonnet Drive opposite Fircrest Drive.
- 56: Bus stop; ledge at asphalt to shelter concrete floor, repairs required. Shelter has no wheelchair space inside allowing shoulder to shoulder seating.
- 57: Crosswalk; traffic control stop sign recommended to stop Blue Bonnet Lane vehicles prior to entering South State Street. Recommend striped crosswalk across Blue Bonnet Lane. Paving should be "tabled" to afford 2% max cross slope through the crosswalk.
- 58: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. Sidewalk should extend from Blue Bonnet Drive opposite Fircrest Drive to near Jefferson Lane at the separated asphalt walkway.
- 59: Crosswalk, just north of Jefferson Lane tee intersection: striping recommended.
- 60: Sidewalk; existing pedestrian travel route is a separated asphalt walkway at varying elevations below the roadway. There are many localized areas that need repaving to correct ledges and lumps from tree root lift, occasional manhole covers, cross slope exceeding 2%, and run slope exceeding 5%.
- 61: Sidewalk; this is an existing traditional concrete curb gutter and sidewalk. Transition at south edge to asphalt walkway (item 60) needs paving corrections for ledges and slopes. There are localized areas with cross slope exceeding 2%.
- 62: Curb ramp; cross slope on approach and top landing exceed 2% max, cross slope on ramp surface exceeds 2% max, ledge at ramp bottom to gutter, no detectable warnings, cross slope at ramp bottom exceeds 2% max; reconstruction required.
- 63: Crosswalk; cross slope exceeds 2% max, paving modifications recommended.
- 64: Curb ramp; cross slope on approach and top landing exceed 2% max, cross slope on ramp surface exceeds 2% max, ledge at ramp bottom to gutter, no detectable warnings, cross slope at ramp bottom exceeds 2% max; reconstruction required.
- 65: Bus stop; insufficient landing for deploying a bus wheelchair lift, need 96" perpendicular to curb direction, no wheelchair seating space (shoulder to shoulder) inside the shelter or adjacent to exterior bench.
- 66: Sidewalk; localized areas with ledges and cross slopes exceeding 2%.



Mendocino County

Location:
Ukiah, South

CalTrans Map & Year:
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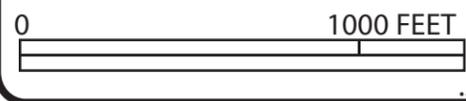


Project #:
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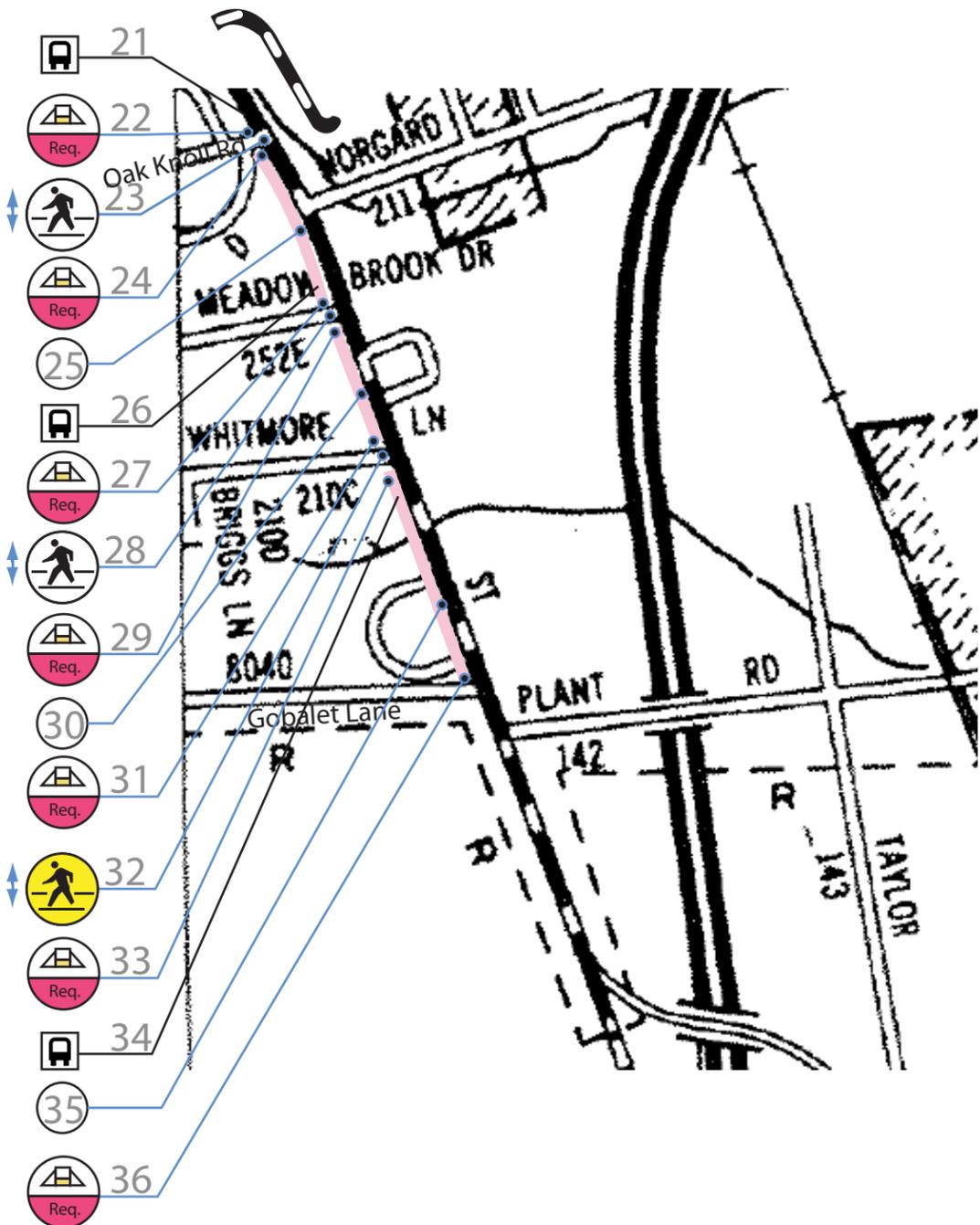
FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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Ukiah, South

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, higher density residential uses, and low density residential tracts; adjoining an incorporated city.



Site Visit Pictures & Notes:

- 21: Bus stop; no provisions for disabled loading, unloading, or connection to compliant travel path; paving improvements required.
- 22: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 23: Crossing; when curb ramps or blended transitions installed at corners, striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 24: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 25: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. The alternative is to construct new traditional style curbs gutters and sidewalks along the road right of way edge requiring significant design and construction expense.
- 26: Bus stop; no provisions for disabled loading, unloading, or connection to compliant travel path; paving improvements required.
- 27: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 28: Crossing; when curb ramps or blended transitions installed at corners, striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 29: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 30: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
- 31: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 32: Crossing; when curb ramps or blended transitions installed at corners, striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
- 33: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
- 34: Bus stop; no provisions for disabled loading, unloading, or connection to compliant travel path; paving improvements required.
- 35: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
- 36: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.



Mendocino County

Location:
Ukiah, South

CalTrans Map & Year:
3H14 1994

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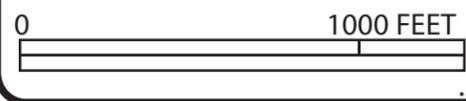


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FUNCTIONAL CLASSIFICATION URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL		RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
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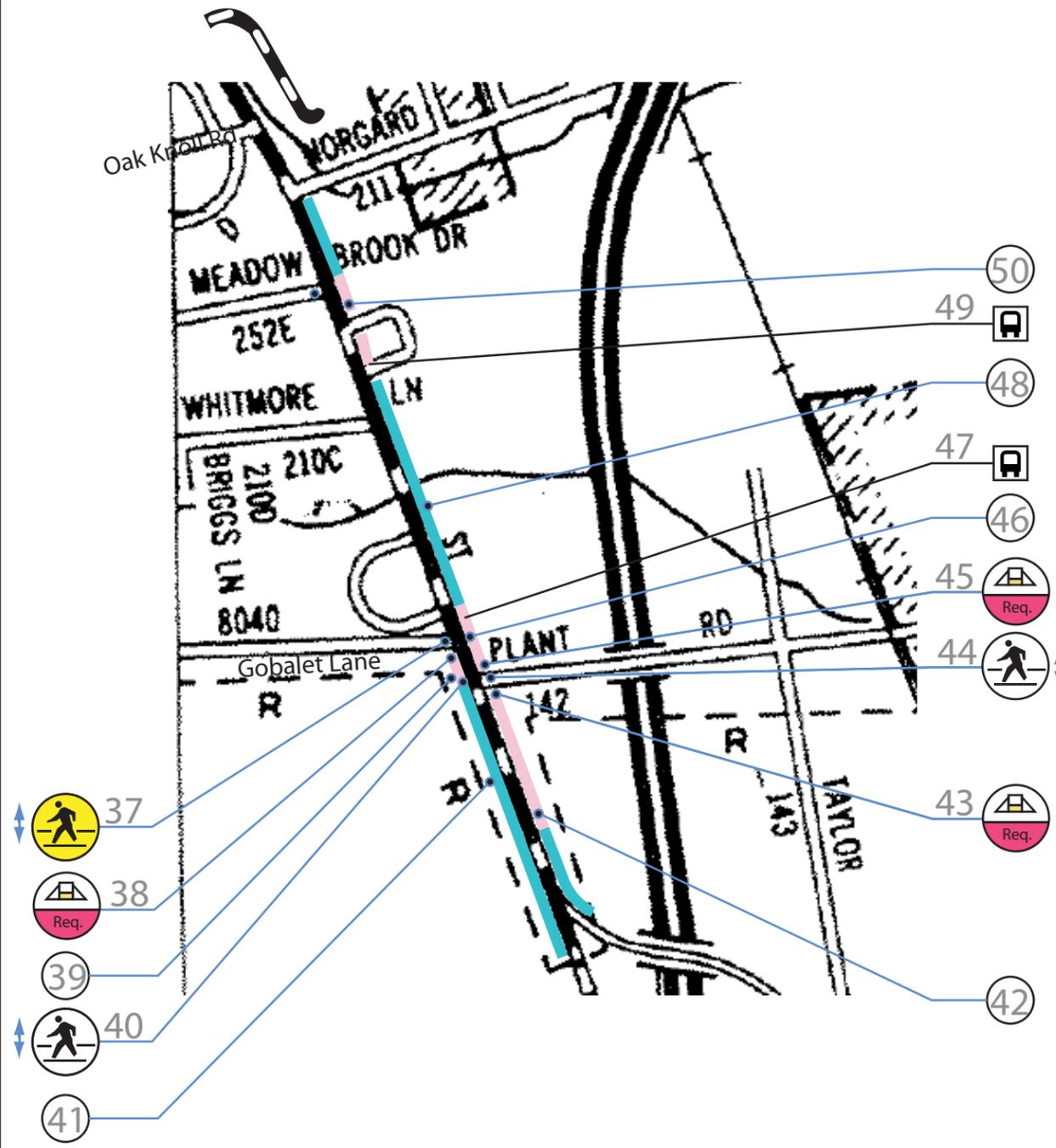
Ukiah, South

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, higher density residential uses, and low density residential tracts; adjoining an incorporated city.

Site Visit Pictures & Notes:

-  37: Crossing; when curb ramps or blended transitions installed at corners, striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
-  38: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
-  39: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
-  40: Crosswalk should be marked at this intersection as areas south on the west road side are rural and undeveloped at this time. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. The intersection has provisions for traffic signal control and pedestrian crossing controls should be installed with the crosswalk. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk.
-  41: The area south of Plant Road is rural. Pedestrian travel paths should be developed with construction projects as they occur.
-  42: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well.
-  43: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
-  44: Crossing; when curb ramps or blended transitions installed at corners, striping recommended. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons.
-  45: Curb ramp or blended transition is missing at this corner and is required for continuous pedestrian and disabled compliant travel path.
-  46: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. Sidewalk should extend from Plant Road to the MTA bus stop.
-  47: Bus stop; no provisions for disabled loading, unloading, or connection to compliant travel path; paving improvements required.
-  48: Sidewalk not required in this area due to undeveloped properties and rural density.
-  49: Bus stop; no provisions for disabled loading, unloading, or waiting adjacent to bench; paving corrections required.
-  50: Sidewalk; existing pedestrian route is not a traditional sidewalk, the only afforded travel route is on the road shoulder, shared with bicycles, and the available route is not striped or otherwise identified. It may be possible to design a pedestrian walking lane along the road shoulder if there is sufficient width for walking and bicycles. Surface water drainage and connections to adjacent property sites must be accommodated in the design as well. Sidewalk should extend from the MTA bus stop to Meadow Brook Drive.



Mendocino County

Location:

Ukiah, South

CalTrans Map & Year:

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Ref. North



Project #: 29075

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OTHER PRINCIPAL ARTERIAL		MINOR ARTERIAL		MAJOR COLLECTOR	
MINOR ARTERIAL COLLECTOR		MINOR COLLECTOR		LOCAL	
LOCAL					

- SW: Existing Non-Compliant Sidewalk	- CR: Not Provided (Required)	- Blended Transition	- Crosswalk Recommended	- Accessible Parking
- SW: Provide Required Sidewalk	- CR: Existing Non-Compliant	- Hazard	- Crosswalk Existing	- Transit Stop
- SW: Existing Compliant Sidewalk	- CR: Existing Compliant	- Reduced Width	- Signage	
- SW: Sidewalk Not Required				

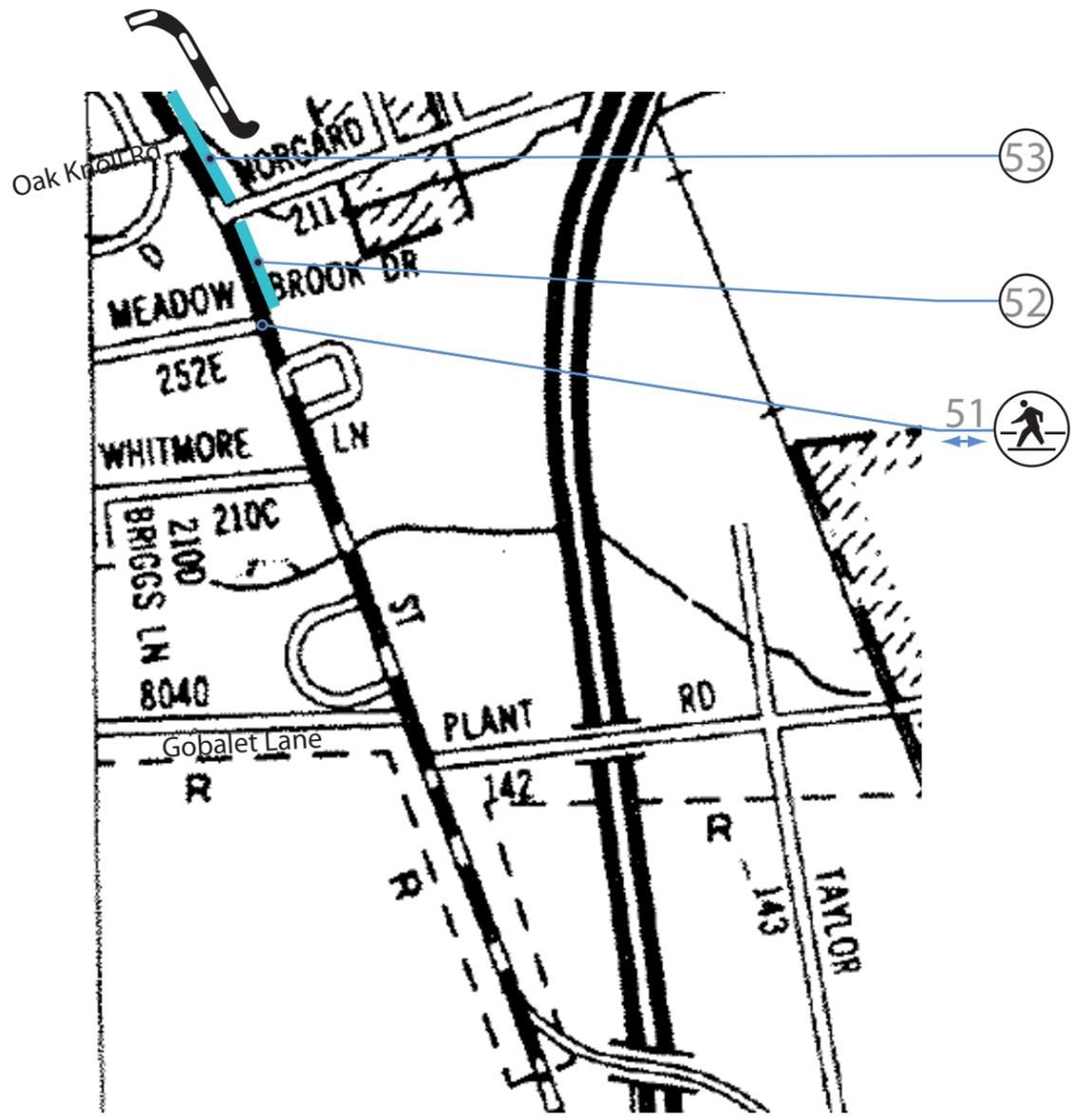
Ukiah, South

Category 1

Site Description:
An unincorporated urban density area with commercial, industrial, civic, higher density residential uses, and low density residential tracts; adjoining an incorporated city.

Site Visit Pictures & Notes:

-  51: Crosswalk should be installed for connection to southbound MTA bus stop across the road. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Due to high traffic it is recommended to have heavy cross bar markings in the crosswalk with high visibility pedestrian paddle signs.
-  52: Sidewalk not required in this area due to undeveloped properties and rural density.
-  53: Sidewalk not required in this area due to undeveloped properties and rural density.



Mendocino County

Location:
Ukiah, South

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	FUNCTIONAL CLASSIFICATION		- SW: Existing Non-Compliant Sidewalk - SW: Provide Required Sidewalk - SW: Existing Compliant Sidewalk - SW: Sidewalk Not Required	- CR: Not Provided (Required) - CR: Existing Non-Compliant - CR: Existing Compliant	- Blended Transition - Hazard - Reduced Width	- Crosswalk Recommended - Crosswalk Existing - Signage	- Accessible Parking - Transit Stop
	URBAN INTERSTATE OR OTHER FREEWAY AND EXPRESSWAY OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL COLLECTOR LOCAL	RURAL INTERSTATE OTHER PRINCIPAL ARTERIAL MINOR ARTERIAL MAJOR COLLECTOR MINOR COLLECTOR LOCAL					

Ukiah, South Photographs: Items 1 - 31



Mendocino County

Location:
Ukiah, South

ADA
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Access Plan

Photographs

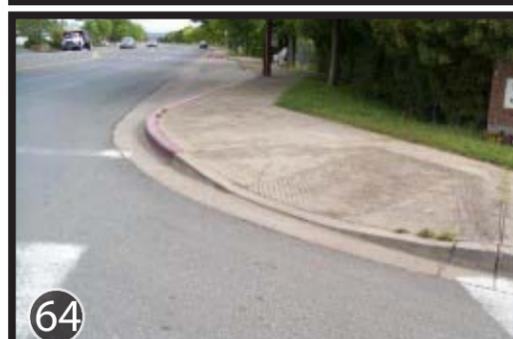
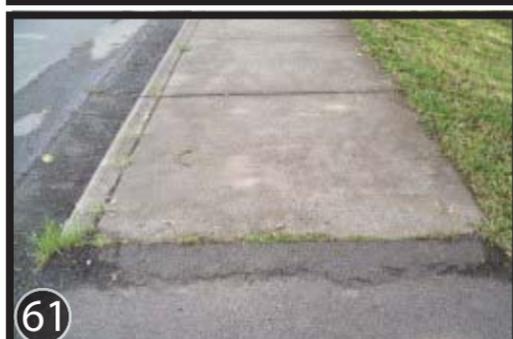


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Date:
06-30-10

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Ukiah, South Photographs: Items 32 - 65



Mendocino County

Location:
Ukiah, South

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Photographs



Project #:
29075

Date:
06-30-10

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Westport

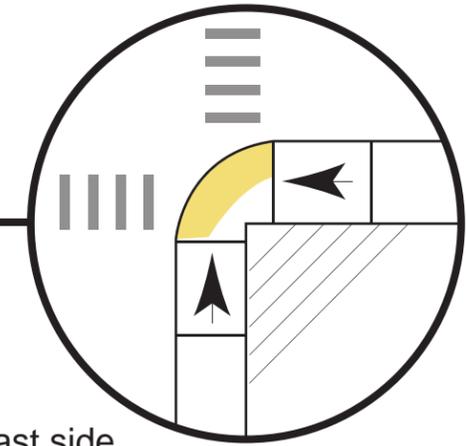
Overview

Category 3

This small coastal community is a recognizable rural town with an urban density area comprised of small lot residences, a couple of commercial buildings, and a small hotel. The village area is primarily residential on smaller sized lots situated mostly on Highway 1 east side. This village is a known visitor serving area for parks and beaches nearby and during peak tourism periods has crowded traffic on Highway 1.

Terrain in the village area is predominately gentle to moderate slopes with some parts on Highway 1 east side trending steeply upward. Current pedestrian infrastructure is the paved asphalt highway shoulder that has constricted width from road lane edge to paving edge between Seaview Drive and Abalone Street. Pedestrian travel paths are achievable along Highway 1.

Current land use zoning restricts opportunities over time to develop more commercial buildings and limits overall development intensity. There will be but a handful of construction development projects over time, mostly comprised of small new buildings and alterations or additions to existing. Pedestrian travel paths serving new or altered facilities should be done with the projects as they develop.



Mendocino
County

Location:
Westport

CalTrans Map & Year:
2F53 1994

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Overview



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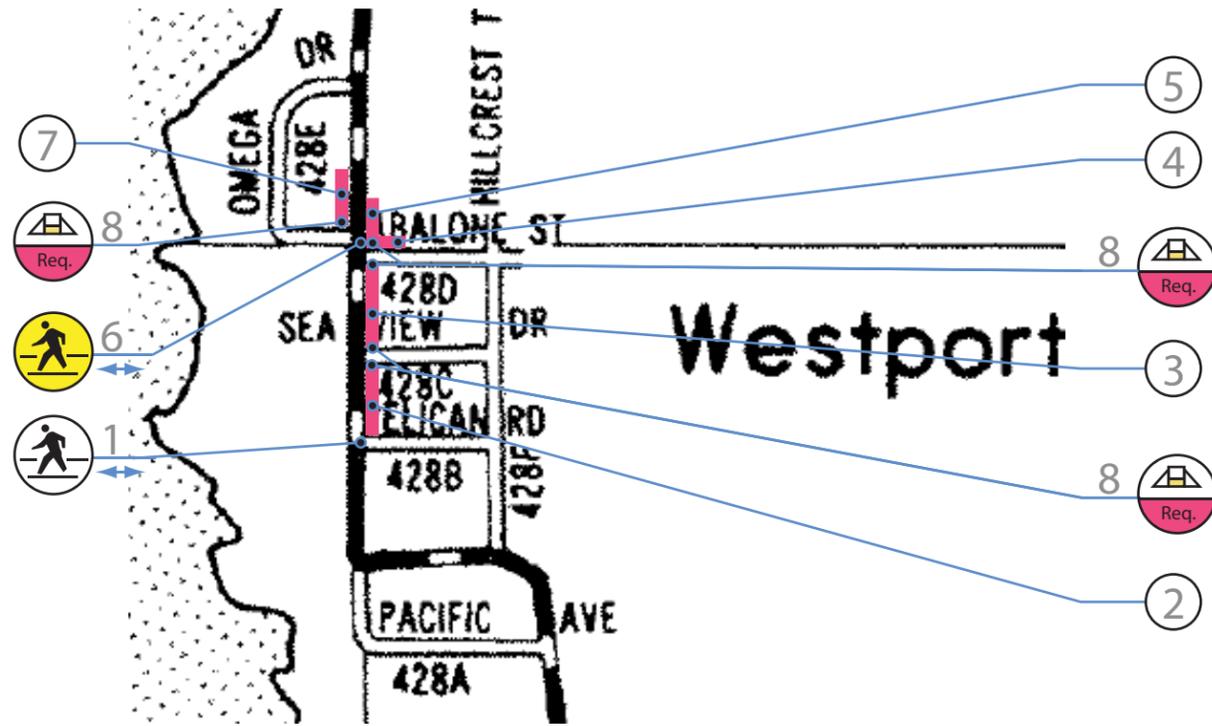
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Westport

Category 3

Site Description:
A visitor serving community situated along Highway 1 with a very small village commercial urban density area along Highway 1.



Site Visit Pictures & Notes:

- 1: Crosswalk is recommended at this intersection to connect pedestrian travel paths with the natural interest site on Highway 1 west side. This recommended crosswalk should be assessed on a warrant basis for pedestrian safety and markings that afford better contrast and route recognition by visually impaired persons. Since there is high traffic during peak tourism times, heavy bar marking with high visibility paddle signs recommended as well.
- 2: Currently the available pedestrian walking path is along the asphalt highway shoulder and has a length with constricted width requiring additional paving. Due to crowded traffic, it is recommended to have a design that separates pedestrians from traffic lanes such as parking lane between walkway and road lane. Design for pedestrian improvements must consider how the pedestrian travel walkway will serve general travel along Highway 1, parked car interferences, vehicle driveways, walkway connections to adjacent business properties, and surface water drainage
- 3: Currently the available pedestrian walking path is along the asphalt highway shoulder with very narrow width from road lane edge to paving edge requiring additional paving. Due to traffic concerns, it is recommended to have a design that separates pedestrians from traffic lanes such as parking lane between walkway and road lane. Design for pedestrian improvements must consider grading to create additional road shoulder width along Highway 1, parked car interferences, vehicle driveways, walkway connections to adjacent properties, and surface water drainage
- 4: This sidewalk portion is necessary for access to the community post office. Grade is moderate to steep and will require a unique solution for disabled pedestrian use and providing the expected parallel parking along the street side.
- 5: Currently the available pedestrian route is asphalt highway shoulder with adjacent parking for the store. Separating pedestrians from the highway lanes and vehicle parking entry/exit should be considered for safety concerns.
- 6: Existing crosswalk; heavy cross bar markings and high visibility paddle signs recommended for this crosswalk due to Highway 1 traffic.
- 7: The pedestrian walking path is along the asphalt highway shoulder. It appears there is sufficient width for a walking lane if parking along this stretch from the corner to the small hotel is either restricted or will have a designated parking lane. Design for pedestrian improvements must consider cross slope grade correction along Highway 1, parked car interferences, vehicle driveways, walkway connections to adjacent properties, and surface water drainage
- 8: Curbramp or blended transition required at corner.



Mendocino County

Location:
Westport

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Potter Valley: Items 1 - 7



Mendocino County

Location:

Potter Valley

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Photographs



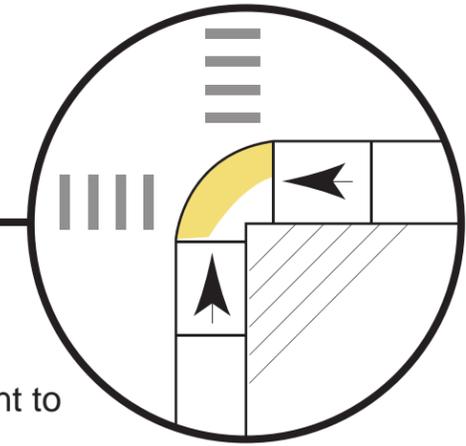
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Willits Area



Overview

Category 1

Mendocino County has jurisdiction for areas surrounding the City of Willits to the north, south, east, and west. The land areas adjacent to the city have distinct location characteristics that are best addressed separately for unique urban density issues.

North Willits

The urban limit per maps is at the city limits and the land is agriculture to the north. Pedestrian travel paths serving new facilities should be done with the projects as they develop and be coordinated with the City for connections.

East Willits

The urban limit shown on maps is close to the city limits. County area within the urban designated area has rural density development at this time. Pedestrian travel paths serving new facilities should be done with the projects as they develop and be coordinated with the City for connections.

West Willits

The urban limit per maps is at the city limits and the land is agriculture beyond. Pedestrian travel paths serving new facilities should be done with the projects as they develop and coordinated with the City for connections.

South Willits

Southwesterly from the city limits is a relatively small residential tract and single lot separately developed homes. Pedestrian infrastructure does not exist throughout the area and is considered a lower priority for improvements. Southerly along US 101, the urban limit is the city limits and there are no urban density developments existing at this time. Pedestrian travel paths serving new facilities should be done with the projects as they develop and be coordinated with the City for connections.



Mendocino
County

Location:

Willits

CalTrans Map & Year:

No Map

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Overview



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